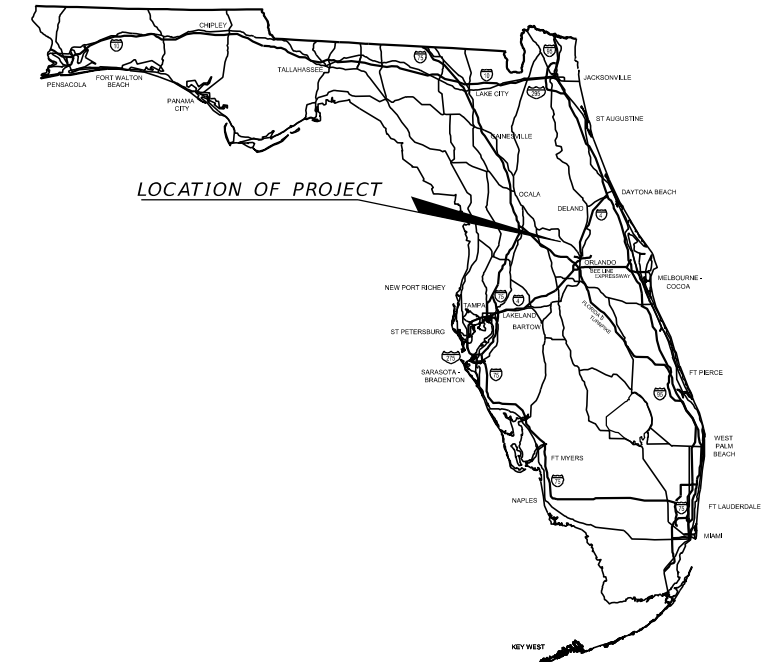


**CENTRAL FLORIDA
EXPRESSWAY AUTHORITY**

**SR 453 (WEKIVA PARKWAY)
FROM SYSTEM INTERCHANGE
TO SR 46**

**LAKE COUNTY
CFX PROJECT NUMBER: 429-206**

SIGNALIZATION PLANS



SHEET NO.	SHEET DESCRIPTION
T-1	KEY SHEET
T-2	TABULATION OF QUANTITIES
T-3	GENERAL NOTES
T-4	SIGNALIZATION PLAN
T-5	INTERCONNECT PLAN
T-6	MAST ARM TABULATION
T-7	GUIDE SIGN WORKSHEET
T-8	STANDARD MAST ARM ASSEMBLIES DATA TABLE
T-9	UTILITY LOCATION PLAN
GT-1	REPORT OF SPT BORINGS

**CENTRAL FLORIDA
EXPRESSWAY AUTHORITY
BOARD MEMBERS**

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---	---

**SIGNALIZATION SHOP DRAWINGS
TO BE SUBMITTED TO:**
JAMES W. HIGHLAND, P.E.
941 LAKE BALDWIN LANE,
ORLANDO, FL 32814
(407) 896-0594

PLANS PREPARED BY:



DRMP, INC.
941 LAKE BALDWIN LANE
ORLANDO, FLORIDA 32814
(407) 896-0594 **CERT. OF AUTH. NO. 2648**

CONTRACT NO. 000860
VENDOR NO. VF591791174001

B.M. DATUM: NAVD-'88

APPROVED _____ DATE / /

**CENTRAL FLORIDA
EXPRESSWAY AUTHORITY**

**SIGNALIZATION PLANS
ENGINEER OF RECORD:** **JAMES W. HIGHLAND, P.E.**

P.E. NO.: **68240**

**PRE-BID SUBMITTAL
OCTOBER 2015**

FISCAL YEAR	SHEET NO.
15	T-1

CFX PROJECT MANAGER: **GLENN PRESSIMONE, P.E.**

TABULATION OF QUANTITIES

PAY ITEM NO.	DESCRIPTION	UNIT	SHEET NUMBERS																				TOTAL THIS SHEET		GRAND TOTAL		REF. SHEET
			T-4		T-5																						
			PLAN	FINAL	PLAN	FINAL	PLAN	FINAL	PLAN	FINAL	PLAN	FINAL	PLAN	FINAL	PLAN	FINAL	PLAN	FINAL	PLAN	FINAL							
630-2-11	CONDUIT (F&I) (OPEN TRENCH)	LF	1457		371																	1828		1828			
630-2-12	CONDUIT (F&I) (DIRECTIONAL BORE)	LF	149																			149		149			
632-7-1	SIGNAL CABLE (F&I)	PI	1																			1		1			
633-2-32	FO CONNECTION (INSTALL) (TERMINATION)	EA			12																	12		12			
633-3-16	FO CONNECTION HARDWARE (F&I)(FIELD-TERM PATCH PANEL)	EA			1																	1		1			
635-2-11	PULL & SPLICE BOX (F&I) (13"X24" STANDARD)	EA	20																			20		20			
635-2-13	PULL & SPLICE BOX (F&I) (30"X60" SPLICE VAULT)	EA			2																	2		2			
639-1-123	SIGNALS - ELEC. POWER SERV.(F&I)(UNDRGRND.)(METER NOT REQ.)	AS	1																			1		1			
639-2-1	SIGNALS - ELECTRICAL SERVICE WIRE (F&I)	LF	813																			813		813			
639-3-11	SIGNALS - ELECTRICAL SERVICE DISCONNECT (F&I) (POLE MOUNT)	EA	1																			1		1			
641-2-12	PRESTRESSED CONCRETE POLE (F&I)(PII SERVICE POLE)	EA	2																			2		2			
646-1-11	ALUMINUM SIGNALS POLE (F&I) (PEDESTAL)	EA	2																			2		2			
649-31-303	MAST ARM ASSEM.(F&I)(110)(SNGL ARM,W/O LUM)(60)	EA	1																			1		1			
649-31-314	MAST ARM ASSEM.(F&I)(110)(DUAL ARM,W/O LUM)(46-46)	EA	1																			1		1			
650-1-14	TRAFFIC SIGNAL (F&I-ALUMINUM) (3 SECT 1 WAY)	AS	8																			8		8			
653-1-11	PEDESTRIAN SIGNAL (F&I LED COUNTDOWN) (1-WAY)	AS	2																			2		2			
660-1-109	LOOP DETECTOR INDUCTIVE (F&I)(TYPE 9,2 Ch,SS,RM)	EA	2																			2		2			
660-1-110	LOOP DETECTOR INDUCTIVE (F&I)(TYPE 10,2 Ch,SS,RM,TD)	EA	1																			1		1			
660-2-102	LOOP ASSEMBLY (F&I) (TYPE B) (6'x6')	AS	10																			10		10			
660-2-106	LOOP ASSEMBLY (F&I) (TYPE F) (6'x40')	AS	2																			2		2			
665-1-11	PEDESTRIAN DETECTOR (F&I) (STANDARD)	EA	2																			2		2			
670-5-110	TRAFFIC CNTL ASSEM (F&I) (NEMA) (PRE NONE)	AS	1																			1		1			
684-1-1	MANAGED FIELD ETHERNET SWITCH (F&I)	EA			1																	1		1			
700-5-22	INTERNALLY ILLUMINATED SIGN (F&I)(OVERHEAD MOUNT)(12-18 FT2)	EA	3																			3		3			

REVISIONS			
DATE	DESCRIPTION	DATE	DESCRIPTION



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DRMP, INC.
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CERTIFICATE OF AUTHORIZATION NO. 2648
JAMES W. HIGHLAND, P.E. LICENSE NO. 68240

CFX PROJ. NO.
429-206

CENTRAL
FLORIDA
EXPRESSWAY
AUTHORITY

*TABULATION OF
QUANTITIES*

SHEET
NO.
T-2

GENERAL NOTES:

1. A PRE-CONSTRUCTION MEETING SHALL BE SCHEDULED WITH LAKE COUNTY TRAFFIC OPERATIONS, (352) 742-1766, PRIOR TO ANY CONSTRUCTION.
2. IT SHALL BE NOTED THAT NO TEST BORINGS BE MADE WHERE CONDUIT RUNS ARE TO BE INSTALLED. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO EXAMINE THE JOB SITE CONDITIONS PRIOR TO SUBMITTING BID PROPOSALS.
3. THE CONTRACTOR SHALL BE RESPONSIBLE FOR NOTIFYING LAKE COUNTY TRAFFIC OPERATIONS, AT 352-742-1766, 48 HOURS IN ADVANCE OF ALL PHASES OF CONSTRUCTION INCLUDING AND NOT LIMITED TO, INSTALLING SIGNAL POLES, GROUND RODS, UNDERGROUND CONDUIT, SIGNAL HEAD ASSEMBLIES, AND LOOP INSTALLATION.
4. DURING CONSTRUCTION, TRAFFIC SHALL BE MAINTAINED IN ACCORDANCE WITH THE FDOT TRAFFIC DESIGN STANDARDS.
5. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ACQUIRING ALL NECESSARY PERMITS INCLUDING THE ELECTRIC PERMIT. THE APPLICATION FOR POWER SHOULD BE COORDINATED WITH LAKE COUNTY TRAFFIC OPERATIONS.
6. THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING THE POWER COMPANY PROVIDING THE ELECTRICAL POWER, TO DETERMINE IF ANY ADDITIONAL FEES ARE REQUIRED TO CONNECT POWER. IF REQUIRED, THE FEE SHALL BE INCLUDED AS PART OF BID ITEM PAYMENT FOR ELECTRICAL SERVICE ASSEMBLY.
7. THE EXACT LOCATIONS OF ALL UTILITIES IS THE RESPONSIBILITY OF THE CONTRACTOR PRIOR TO BEGINNING OF CONSTRUCTION.
8. THE CONTRACTOR SHALL STAKE ALL POLE LOCATIONS AND HAVE IT APPROVED BY LAKE COUNTY TRAFFIC OPERATIONS.
9. THE CONTRACTOR SHALL NOTIFY THE APPROPRIATE UTILITY DEPARTMENT AT LEAST 48 HOURS IN ADVANCE OF POLE SETTING OPERATIONS WHERE A CONFLICT WITH OVERHEAD ELECTRICAL CONDUCTORS IS EXPECTED AND WHEN JOINT USE POLES ARE TO BE USED.
10. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ESTABLISHING THE PROPER AMOUNT OF RAKE WITH RESPECT TO LOADING TO BE USED ON POLES AT THE TIME OF INSTALLATION.
11. ALL MATERIALS AND HARDWARE SHALL BE F.D.O.T. APPROVED, AND PRE-APPROVED BY LAKE COUNTY TRAFFIC OPERATIONS.
12. ANY STRIPING/PAVEMENT MARKINGS OR LANDSCAPING DESTROYED BY THE CONTRACTOR DURING CONSTRUCTION SHALL BE RESTORED AT THE CONTRACTOR'S EXPENSE.
13. THE COST FOR THE USE OF "TRAFFIC CONTROL OFFICER" IN THE EVENT OF LANE CLOSURES SHALL BE INCIDENTAL TO THE WORK AND WILL NOT BE PAID SEPARATELY.
14. THE CONTRACTOR SHALL FURNISH LAKE COUNTY TRAFFIC OPERATIONS WITH EMERGENCY CONTACTS AND PHONE NUMBERS AND HAVE AN IMSA LEVEL II CERTIFIED SIGNAL TECHNICIAN ON CALL WITHIN A TWO HOUR MAXIMUM RESPONSE TIME.

LOOPS/PULL BOXES:

1. PULL BOX COVERS SHALL BE FDOT APPROVED NON METALLIC WITH RECESSED COVER LOGO "TRAFFIC SIGNAL" OR "FIBER OPTICS" AS APPROPRIATE.
2. PULL BOXES SHALL BE LOCATED AWAY FROM EDGE OF PAVEMENT, OR BEHIND A CURB WHEN POSSIBLE, ATTEMPTING TO KEEP FROM THE BOTTOM OF A DITCH OR RETENTION AREA, AND WHERE LARGE VEHICLES PARK.
3. MULTIPLE CONDUITS IN THE SAME RUN SHALL BE PAID FOR AS DIRECTIONAL BORED OR JACK AND BORED FOR THE FIRST CONDUIT ONLY. ALL OTHERS WILL BE PAID FOR AS UNDERGROUND. ALL FIBER OR FUTURE USE CONDUITS SHALL HAVE A LOCATE WIRE INSTALLED.
4. ALL FIBER OR FUTURE USE CONDUIT SHALL HAVE A LOCATE WIRE INSTALLED.
5. LOOP HOME RUNS SHALL NOT BE CUT INTO THE RADIUS.

CABINET/CONTROLLER:

1. THE CONTROLLER ASSEMBLY SHALL BE A "LAKE COUNTY" NAZTEC REGIONAL TS2-1; WITH A TS2-2 CONTROLLER ETHERNET ENABLED FOR COUNTY CLOSED LOOP SYSTEM, TO INCLUDE A GENERATOR SWITCH BOX PANEL.
2. A GENERATOR PAD, 3'X3'X4" CONCRETE SHALL BE INSTALLED WITH 5/8" EYEBOLT INSTALLED IN THE PAD ADJACENT TO CABINET BASE.
3. THE CABINET CONCRETE BASE SHALL BE A MINIMUM OF 32" x 48" TO ACCOMMODATE TYPE 6 CABINETS.
4. GROUNDING FOR THE CONTROLLER ASSEMBLY SHALL MEASURE 25 OHMS. OR LESS.
5. THE CABINET DOOR SHALL OPEN AWAY FROM THE INTERSECTION WHEN POSSIBLE.
6. THE FIRST BUFFER (BLUE BUFFER) OF SINGLE MODE FIBER SHALL BE TERMINATED IN THE CABINET USING SC CONNECTORS.
7. THE MOUNTING OF THE ELECTRICAL SERVICE TO THE TRAFFIC SIGNAL CABINET SHALL BE PROHIBITED.
8. A MANUAL PUSH BUTTON CORD SHALL BE FURNISHED IN ALL CONTROLLER CABINETS.

SIGNAL HEADS:

1. SIGNAL HEADS SHALL BE WIRED DIRECTLY TO THE TERMINAL BLOCKS. THE USE OF "JONES" PLUGS IS PROHIBITED.
2. DISCONNECTS SHALL OPEN FROM THE SIGNAL FACE. (NOT FROM THE REAR)
3. SIGNAL HEADS SHALL BE WIRED PER IMSA NEMA PHASING, PHASES 2 AND 6 ARE TYPICALLY ASSIGNED TO MAJOR-STREET WITH PHASE 2 BEING SOUTH OR WEST BOUND THROUGH MOVEMENTS. THE USUAL CONVENTION IS FOR THROUGH PHASES TO BE NUMBERED IN THE CLOCKWISE DIRECTION STARTING WITH PHASE 2, AND THE LEFT-TURN PHASES TO BE NUMBERED IN THE CLOCKWISE DIRECTION, WITH PHASE 1 BEING ACCOMPANYING LEFT-TURN TO PHASE 6.
4. VEHICLE SIGNAL HEAD ASSEMBLIES SHALL BE BLACK, CAST ALUMINUM, WITH TUNNEL VISORS, AND LED'S FOR ALL INDICATIONS UNLESS OTHERWISE NOTED.
5. PEDESTRIAN SIGNAL ASSEMBLIES SHALL BE CAST ALUMINUM, WITH FULL HAND/PERSON LED'S WITH COUNTDOWN INDICATION.
6. ALL PEDESTRIAN SIGNAL HEADS SHALL HAVE LOUVERS AND BE COUNTDOWN LED SIGNALS WITH THE PROPER COUNTDOWN PEDESTRIAN SIGNAGE. ALL PEDESTRIAN SIGNALS NOT MOUNTED ON SIGNAL POLES OR MAST ARM POLES SHALL BE THE BREAKAWAY TYPE COMMONLY REFERRED TO AS A "T-BASE". THIS T-BASE WILL HAVE AN ACCESS DOOR FOR WIRING AND MAINTENANCE.

SIGNAL HEADS (CONT'D):

7. SIGNAL CABLE SHALL BE ATTACHED TO MESSENGER WIRE USING PROPER SIZE SPIRAL WRAP.
8. FDOT OPTION OF PIVOTAL ADJUSTABLE HANGERS ASSEMBLY WITH FLAT EXTENSION BARS SHALL BE USED INSTEAD OF DROP PIPES FOR DUEL POINT ATTACHMENTS.

INTERNALLY ILLUMINATED STREET NAME SIGNS:

1. INTERNALLY ILLUMINATED STREET NAME SIGNS (LED) SHALL BE POWERED BY A SEPARATE CIRCUIT BREAKER AND BE DESIGNED AND INSTALLED IN ACCORDANCE WITH LAKE COUNTY PUBLIC WORKS INTERNALLY ILLUMINATED STREET NAME SIGN DETAIL. A PHOTOCCELL SHALL BE INSTALLED NEAR THE ELECTRIC SERVICE WITHIN REACH OF A LIFT TRUCK. CARE TO BE GIVEN TO INSTALL WHERE STREET LIGHTING DOES NOT AFFECT OPERATION.
2. THE CONTRACTOR SHALL FURNISH LAKE COUNTY TRAFFIC OPERATIONS, TWO COMPLETE SETS OF AS-BUILT PLANS THAT INCLUDE CONDUIT AND PULL BOX LOCATIONS, AT FINAL INSPECTION.
3. STREET NAME SIGNS SHALL BE MOUNTED FREE SWINGING BELOW MAST ARM W/ 2-POINT ATTACHMENT.

PAY ITEM NOTES:

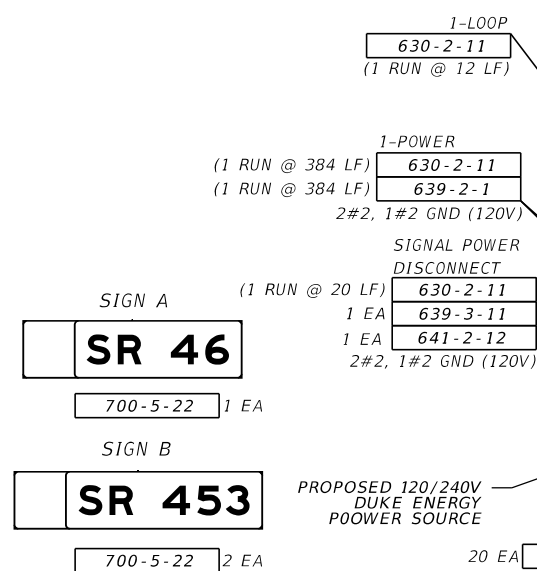
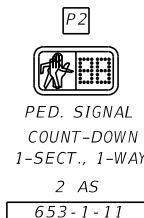
- 632-7-1: SIX (6) FEET OF ADDITIONAL SLACK CABLE STORED IN THE UPRIGHT IS INCIDENTAL TO THIS PAY ITEM NUMBER.
- 639-1-123: ALL FEES BY THE POWER COMPANY FOR ENERGIZING THE POWER SERVICE AND A SEPARATE BREAKER FOR THE ILLUMINATED SIGNS IS INCIDENTAL TO THIS PAY ITEM NUMBER. CONTRACTOR SHALL COORDINATE WITH LAKE COUNTY TRAFFIC OPERATIONS TO SET UP AN ACCOUNT FOR THE NEW POWER SOURCE.
- 665-1-11: FTP-68B-06 SIGNS ARE INCIDENTAL TO THIS PAY ITEM NUMBER.
- 670-5-110: THE CONCRETE GENERATOR PAD IS INCIDENTAL TO THIS PAY ITEM NUMBER.

REVISIONS				 DRMP <small>ENGINEERS • SURVEYORS • PLANNERS • SCIENTISTS</small> <small>DRMP, INC.</small> <small>941 LAKE BALDWIN LANE, ORLANDO, FLORIDA 32814</small> <small>PHONE: (407) 896-0594 FAX: (407) 896-4836</small> <small>CERTIFICATE OF AUTHORIZATION NO. 2648</small> <small>JAMES W. HIGHLAND, P.E. LICENSE NO. 68240</small>	CFX PROJ. NO.	CENTRAL FLORIDA EXPRESSWAY AUTHORITY	GENERAL NOTES	SHEET NO.
DATE	DESCRIPTION	DATE	DESCRIPTION					
				429-206			T-3	

DETECTORS FOR LOOPS				
LOOP	NO. OF LOOPS	NO. OF NEW DETS.	CHANNEL NO.	DELAY TIME (SEC)
L-8R	1	1	1	5
L-8	1	1	2	
L-6A,B,C	3	1	1	
L-6D,E,F	3	1	2	
L-2A,B	2	1	1	
L-2C,D	2	1	2	

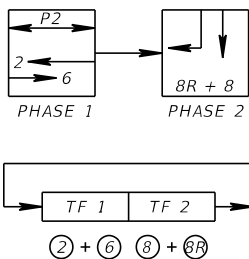
DELAY TIME IS INITIAL AND MAY REQUIRE FIELD ADJUSTING AS DIRECTED BY PROJECT ENGINEER.

PEDESTRIAN HEAD DETAILS



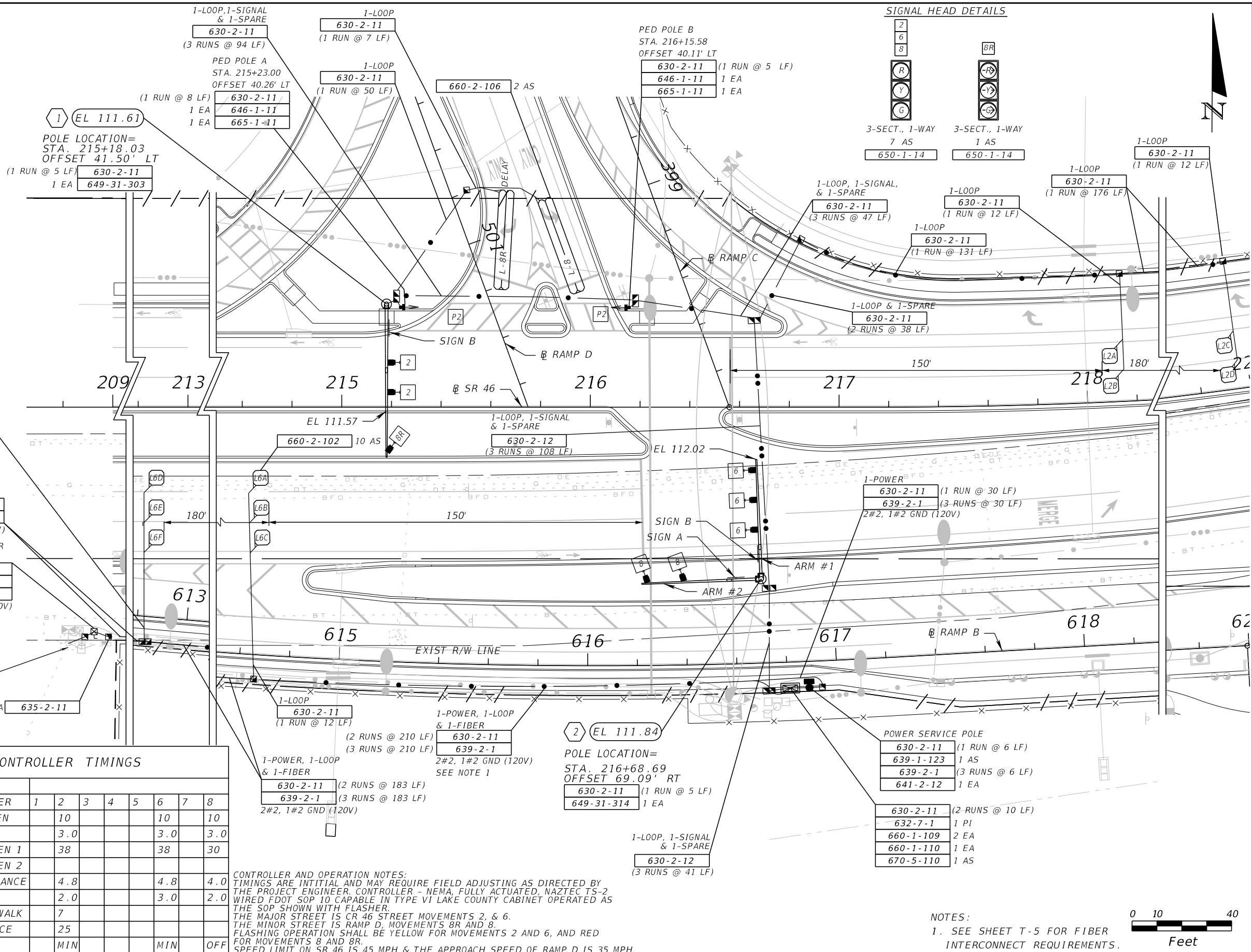
(SEE GUIDE SIGN WORKSHEET FOR DETAILS)

MODIFIED S.O.P. 1

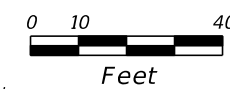


CONTROLLER TIMINGS								
TIMING FUNCTION	1	2	3	4	5	6	7	8
MINIMUM GREEN		10				10		10
EXTENSION		3.0				3.0		3.0
MAXIMUM GREEN 1		38				38		30
MAXIMUM GREEN 2								
YELLOW CLEARANCE		4.8				4.8		4.0
ALL RED		2.0				3.0		2.0
PEDESTRIAN WALK		7						
PED. CLEARANCE		25						
RECALL		MIN				MIN		OFF

CONTROLLER AND OPERATION NOTES:
 TIMINGS ARE INITIAL AND MAY REQUIRE FIELD ADJUSTING AS DIRECTED BY THE PROJECT ENGINEER. CONTROLLER - NEMA, FULLY ACTUATED, NAZTEC TS-2 WIRED FDOT SOP 10 CAPABLE IN TYPE VI LAKE COUNTY CABINET OPERATED AS THE SOP SHOWN WITH FLASHER.
 THE MAJOR STREET IS CR 46 STREET MOVEMENTS 2, & 6.
 THE MINOR STREET IS RAMP D, MOVEMENTS 8R AND 8.
 FLASHING OPERATION SHALL BE YELLOW FOR MOVEMENTS 2 AND 6, AND RED FOR MOVEMENTS 8 AND 8R.
 SPEED LIMIT ON SR 46 IS 45 MPH & THE APPROACH SPEED OF RAMP D IS 35 MPH.



NOTES:
 1. SEE SHEET T-5 FOR FIBER INTERCONNECT REQUIREMENTS.



REVISIONS			
DATE	DESCRIPTION	DATE	DESCRIPTION

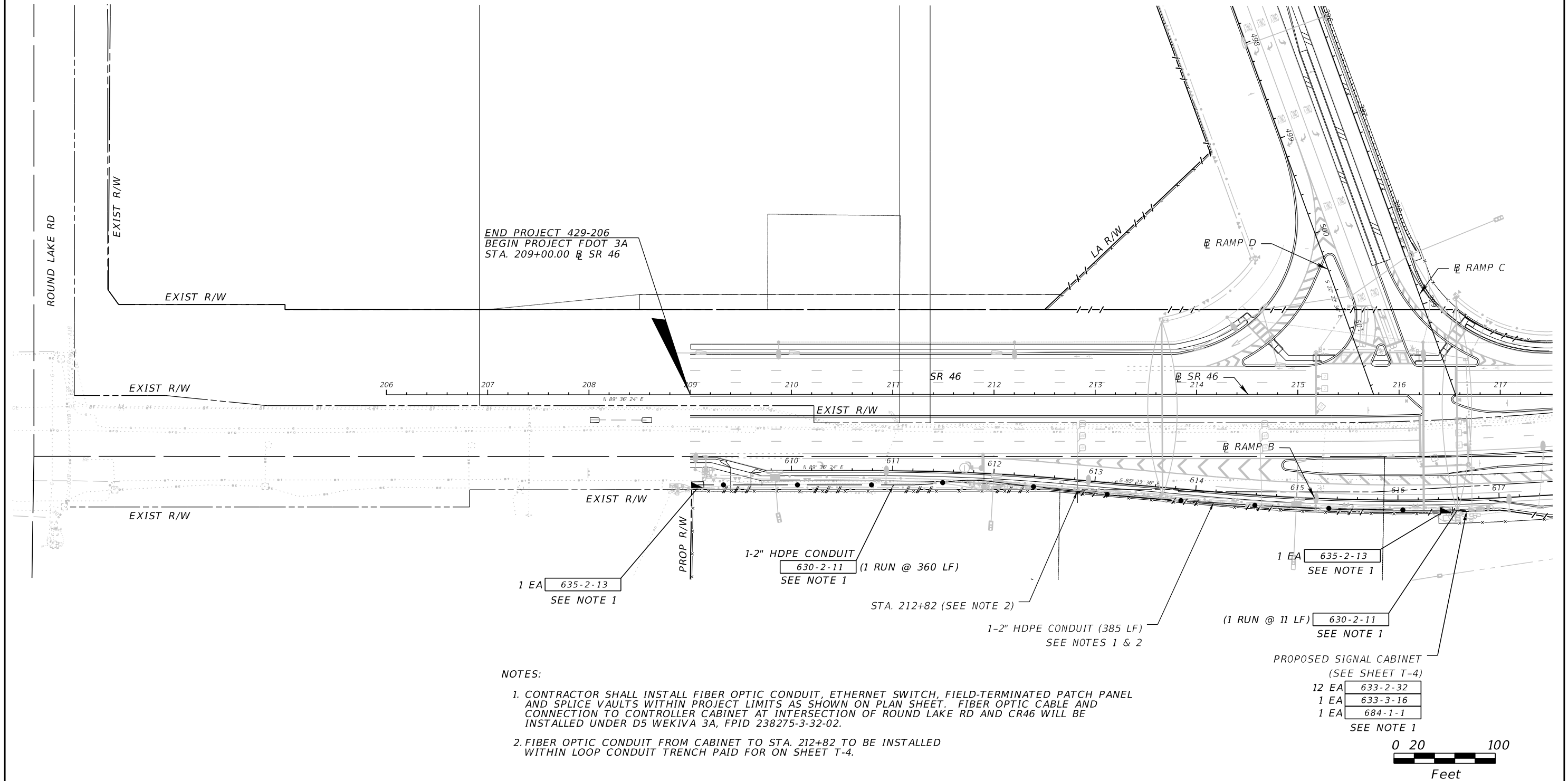
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 429-206

CENTRAL FLORIDA EXPRESSWAY AUTHORITY

SIGNALIZATION PLAN

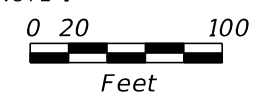
SHEET NO.
 T-4



NOTES:

1. CONTRACTOR SHALL INSTALL FIBER OPTIC CONDUIT, ETHERNET SWITCH, FIELD-TERMINATED PATCH PANEL AND SPLICE VAULTS WITHIN PROJECT LIMITS AS SHOWN ON PLAN SHEET. FIBER OPTIC CABLE AND CONNECTION TO CONTROLLER CABINET AT INTERSECTION OF ROUND LAKE RD AND CR46 WILL BE INSTALLED UNDER D5 WEKIVA 3A, FPID 238275-3-32-02.
2. FIBER OPTIC CONDUIT FROM CABINET TO STA. 212+82 TO BE INSTALLED WITHIN LOOP CONDUIT TRENCH PAID FOR ON SHEET T-4.

- PROPOSED SIGNAL CABINET
(SEE SHEET T-4)
- 12 EA 633-2-32
 - 1 EA 633-3-16
 - 1 EA 684-1-1
- SEE NOTE 1



REVISIONS			
DATE	DESCRIPTION	DATE	DESCRIPTION


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INTERCONNECT PLAN

SHEET NO.
T-5

STANDARD MAST ARM ASSEMBLIES DATA TABLE

Table Date 01-01-12

STRUCTURE ID NUMBERS	ASSEMBLY NUMBERS ⁽¹⁾	FIRST ARM			SECOND ARM			UF (deg)	LL (deg)	POLE			SPECIAL DRILLED SHAFT ⁽⁴⁾						
		ARM TYPE	FAA ⁽²⁾ (ft.)	FBA ⁽²⁾ (in.)	ARM TYPE	FAA ⁽²⁾ (ft.)	FBA ⁽²⁾ (in.)			POLE TYPE	UAA ⁽³⁾ (ft.)	UB (ft.)	UCA ⁽³⁾ (in.)	DA (ft.)	DB (ft.)	RA	RB	RC	RD (in.)
1	E5-T3	E5								T3	23	20	15.81	20	4.50	11	15	10	12
2	E4-E4-T3	E4			E4			90		T3	23	20	15.81	20	4.50	11	15	10	12

TABLE NOTES [Notes Date 07-01-14]:

1. Assembly Number Legend

Single Arm:

Arm Type - Pole Type = D# - S#
= E# - T#

Double Arm:

First Arm Type - Second Arm Type - Pole Type = D# - D# - S#
= E# - E# - T#

2. If an entry appears in columns "FAA" and "FBA", a shorter arm is required. This is obtained by removing length from the arm tip. For these cases the mast arm length shall be shortened from "FA" to "FAA" and the tip diameter shall be increased from "FB" to "FBA".

3. If an entry appears in columns "UAA" and "UCA", a shorter pole is required. This is obtained by removing length from the pole tip. For these cases the pole height shall be shortened from "UA" to "UAA" and the pole tip diameter shall be increased from "UC" to "UCA".

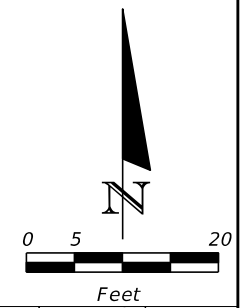
4. The foundations for Standard Mast Arm Assemblies are pre-designed and are based upon the following conservative soil criteria which covers the great majority of soil types found in Florida. Only complete the "Special Drilled Shaft" data information if site conditions dictate drilled shafts with additional foundation capacity.

Classification = Cohesionless (Fine Sand)
Friction Angle = 30 Degrees (30°)
Unit Weight = 50 lbs. / cu. ft. (assumed saturated)
N-blowcount = 15

GENERAL NOTES:

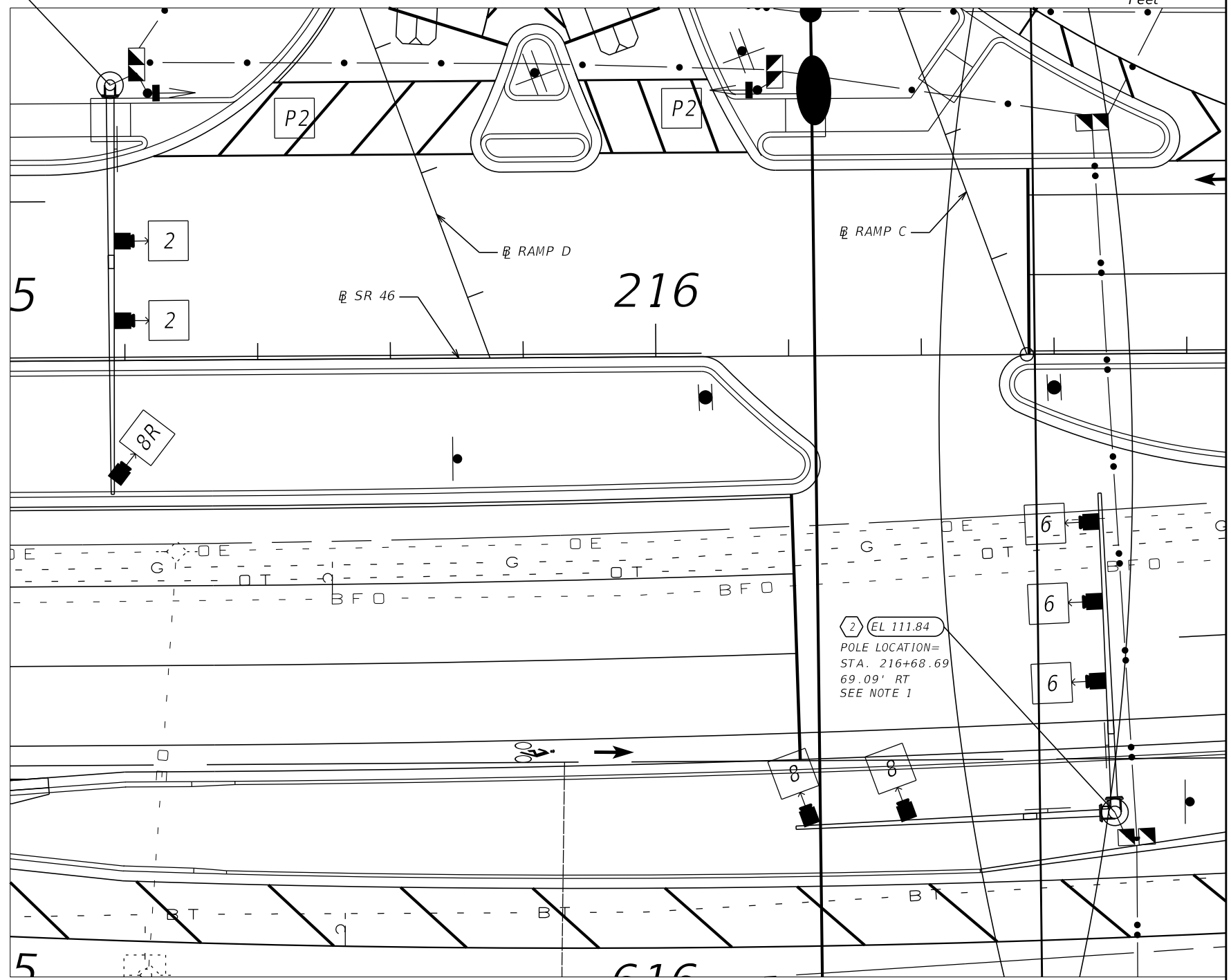
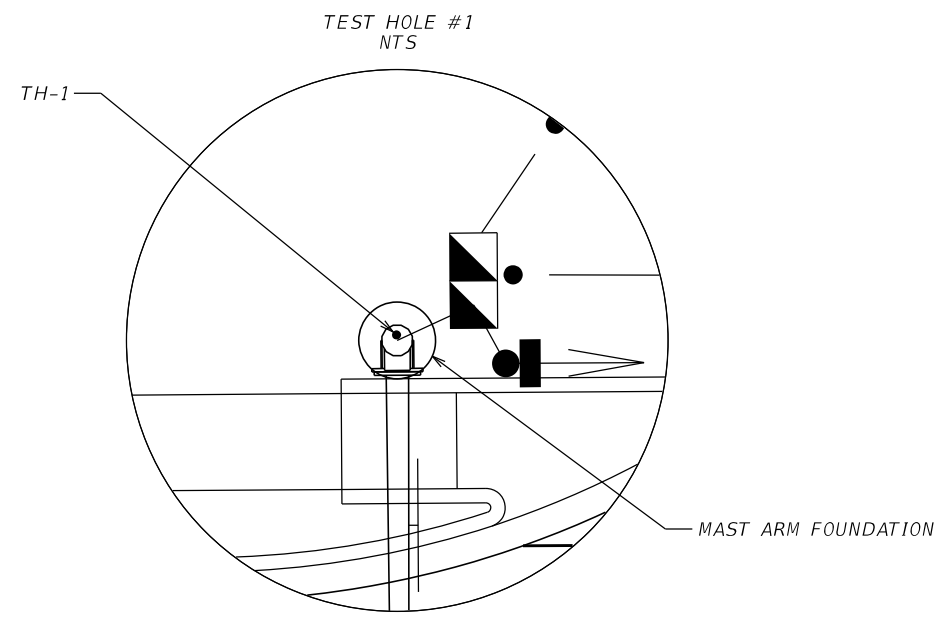
1. Work this sheet with the Signal Designer's "Mast Arm Tabulation". See "Mast Arm Tabulation" for special instructions that include non-standard Handhole location, paint color, terminal compartment requirement, and pedestrian features.
2. Work with Index Nos. 17743 and 17745.

REVISIONS				AVCON, INC. ENGINEERS & PLANNERS 5555 EAST MICHIGAN STREET, SUITE 200 ORLANDO, FL 32822-2779 OFFICE: (407) 599-1122 CORPORATE CERTIFICATE OF AUTHORIZATION No: 5057 EOR: Luca DeVerme P.E. 63055	CFX PROJ. NO. 429-206	CENTRAL FLORIDA EXPRESSWAY AUTHORITY	STANDARD MAST ARM ASSEMBLIES DATA TABLE	SHEET NO.
DATE	DESCRIPTION	DATE	DESCRIPTION					T-8



1 EL 111.61
POLE LOCATION
STA. 215+18.03
41.50' LT

TEST HOLE NO.	UTILITY DESCRIPTION (TYPE, OWNER)	SIZE	MATERIAL	E AND/OR C			UTILITY DEPTH	COMMENTS
				STATION	OFFSET	LT/RT		
TH-1	N/A	N/A	N/A	215+18	41.50'	LT	N/A	CLEAR



2 EL 111.84
POLE LOCATION=
STA. 216+68.69
69.09' RT
SEE NOTE 1

NOTES:
1. PROPOSED MAST ARM IS LOCATED WITHIN THE EXISTING SR 46 ROADWAY. FOUNDATION HAS BEEN ASSUMED CLEAR OF UTILITY CONFLICTS BASED ON PROVIDED UTILITY MARKOUTS AND FIELD VERIFICATIONS. NO TEST HOLE WAS PERFORMED.

REVISIONS			
DATE	DESCRIPTION	DATE	DESCRIPTION

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CENTRAL
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AUTHORITY

UTILITY LOCATION PLAN

SHEET NO.
T-9

