

**CENTRAL FLORIDA EXPRESSWAY AUTHORITY (CFX)**

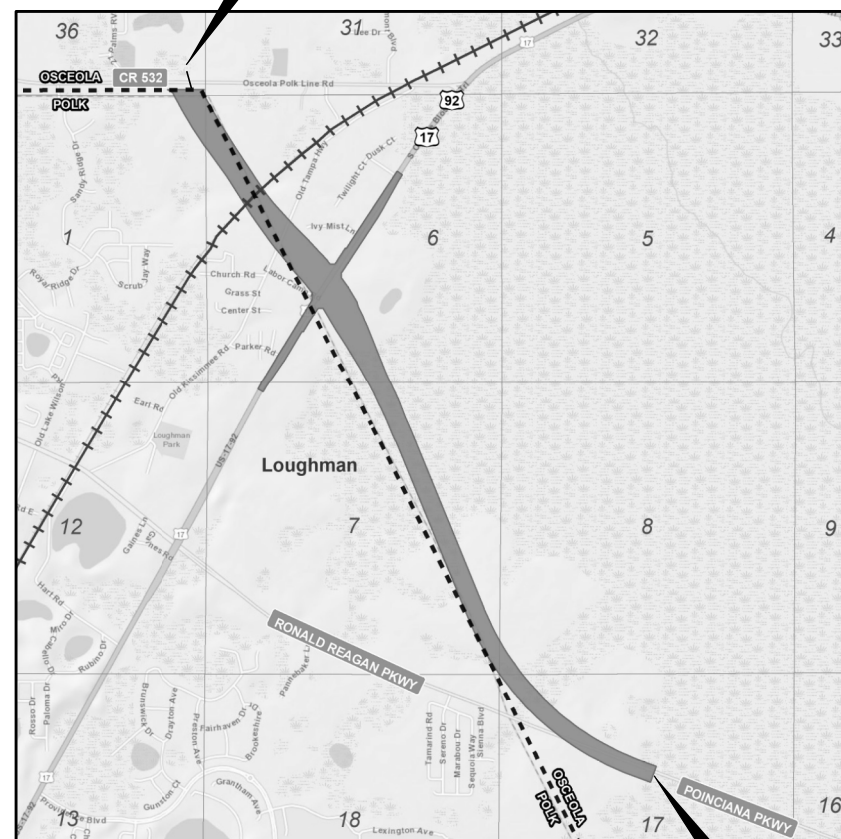
**TYPICAL SECTION PACKAGE**

OSCEOLA COUNTY  
POLK COUNTY

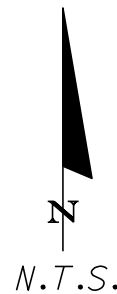
STATE ROAD NO. 538  
SR 538 POINCIANA PARKWAY EXTENSION PD&E STUDY  
(FROM CR 532 TO RONALD REAGAN PARKWAY)

CFX PROJECT NO. 599-224A

BEGIN PROJECT  
STA. 32+30.98



END PROJECT  
STA. 200+75.00



THIS DOCUMENT HAS BEEN DIGITALLY  
SIGNED AND SEALED BY:

PRINTED COPIES OF THIS DOCUMENT ARE  
NOT CONSIDERED SIGNED AND SEALED.  
THE SIGNATURE MUST BE VERIFIED  
ON THE ELECTRIC DOCUMENTS.

KIMLEY-HORN  
189 SOUTH ORANGE AVE., SUITE 1000  
ORLANDO, FLORIDA 32801  
TEL: 407-898-1511  
CERTIFICATE OF AUTHORIZATION 696  
VENDOR NO:  
FRED BURKETT, P.E. NO. 45825

THE ABOVE NAMED PROFESSIONAL ENGINEER SHALL  
BE RESPONSIBLE FOR THE FOLLOWING SHEETS IN  
ACCORDANCE WITH RULE 6G15-23.004 F.A.C.

TYPICAL SECTION PACKAGE

| SHEET NO. | SHEET DESCRIPTION      |
|-----------|------------------------|
| 1         | COVER SHEET            |
| 2         | TYPICAL SECTION NO. 1  |
| 3         | TYPICAL SECTION NO. 2  |
| 4         | TYPICAL SECTION NO. 3  |
| 5         | TYPICAL SECTION NO. 4  |
| 6         | TYPICAL SECTION NO. 5  |
| 7         | TYPICAL SECTION NO. 6  |
| 8         | TYPICAL SECTION NO. 7  |
| 9         | TYPICAL SECTION NO. 8  |
| 10        | TYPICAL SECTION NO. 9  |
| 11        | TYPICAL SECTION NO. 10 |

SHEET  
NO.

1

# PROJECT CONTROLS

# TYPICAL SECTION NO. 1 SR 538

## CONTEXT CLASSIFICATION

- ( ) C1 : NATURAL      ( ) C3C : SUBURBAN COMM.
- ( ) C2 : RURAL        ( ) C4 : URBAN GENERAL
- ( ) C2T : RURAL TOWN   ( ) C5 : URBAN CENTER
- ( ) C3R : TOWN        ( ) C6 : URBAN CORE
- (X) N/A : LA FACILITY

## FUNCTIONAL CLASSIFICATION

- ( ) INTERSTATE        ( ) MAJOR COLL.
- (X) FREEWAY/EXPWY.   ( ) MINOR COLL.
- ( ) PRINCIPAL ARTERIAL   ( ) LOCAL
- ( ) MINOR ARTERIAL

## HIGHWAY SYSTEM

- ( ) NATIONAL HIGHWAY SYSTEM
- ( ) STRATEGIC INTERMODAL SYSTEM
- (X) STATE HIGHWAY SYSTEM
- ( ) OFF STATE HIGHWAY SYSTEM

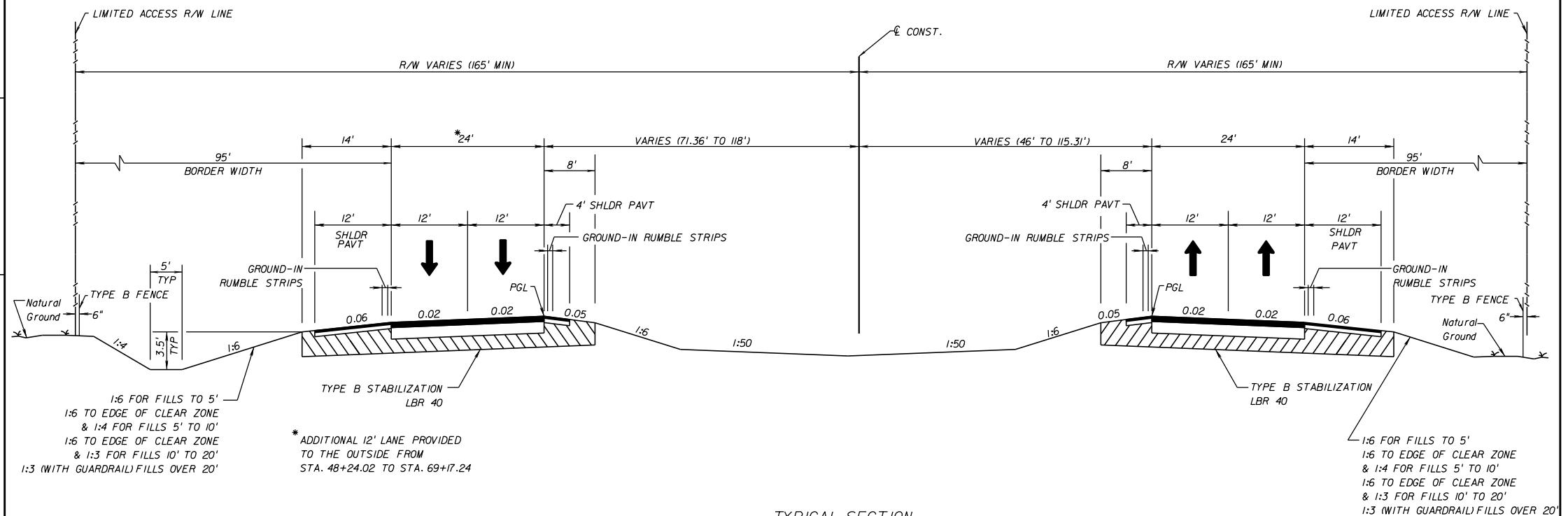
## ACCESS CLASSIFICATION

- (X) 1 - FREEWAY
- ( ) 2 - RESTRICTIVE W/Service Roads
- ( ) 3 - RESTRICTIVE W/660 ft. Connection Spacing
- ( ) 4 - NON-RESTRICTIVE W/2640 ft. Signal Spacing
- ( ) 5 - RESTRICTIVE W/440 ft. Connection Spacing
- ( ) 6 - NON-RESTRICTIVE W/1320 ft. Signal Spacing
- ( ) 7 - BOTH MEDIAN TYPES

## CRITERIA

- (X) NEW CONSTRUCTION / RECONSTRUCTION
- ( ) RESURFACING (LA FACILITIES)
- ( ) RRR (ARTERIALS & COLLECTORS)

## POTENTIAL EXCEPTIONS AND VARIATIONS RELATED TO TYPICAL SECTION:



TYPICAL SECTION  
SR 538  
STA 46+65.53 TO STA 73+11.38

## TRAFFIC DATA

CURRENT YEAR = 2019 AADT = N/A  
 ESTIMATED OPENING YEAR = 2025 AADT = 22,600  
 ESTIMATED DESIGN YEAR = 2045 AADT = 28,000  
 K = 11% D = 60% T (24 HOUR) = 6%  
 DESIGN SPEED = 70 MPH  
 POSTED SPEED = 65 MPH

SHEET NO.

2

THE OFFICIAL RECORD OF THIS SHEET IS THE ELECTRONIC FILE DIGITALLY SIGNED AND SEALED UNDER RULE 61G15-23.004, F.A.C.

# PROJECT CONTROLS

# TYPICAL SECTION NO.2 SR 538

## CONTEXT CLASSIFICATION

- ( ) C1 : NATURAL ( ) C3C : SUBURBAN COMM.
- ( ) C2 : RURAL ( ) C4 : URBAN GENERAL
- ( ) C2T : RURAL TOWN ( ) C5 : URBAN CENTER
- ( ) C3R : TOWN ( ) C6 : URBAN CORE
- (X) N/A : LA FACILITY

## FUNCTIONAL CLASSIFICATION

- ( ) INTERSTATE ( ) MAJOR COLL.
- (X) FREEWAY/EXPWY. ( ) MINOR COLL.
- ( ) PRINCIPAL ARTERIAL ( ) LOCAL
- ( ) MINOR ARTERIAL

## HIGHWAY SYSTEM

- ( ) NATIONAL HIGHWAY SYSTEM
- ( ) STRATEGIC INTERMODAL SYSTEM
- (X) STATE HIGHWAY SYSTEM
- ( ) OFF STATE HIGHWAY SYSTEM

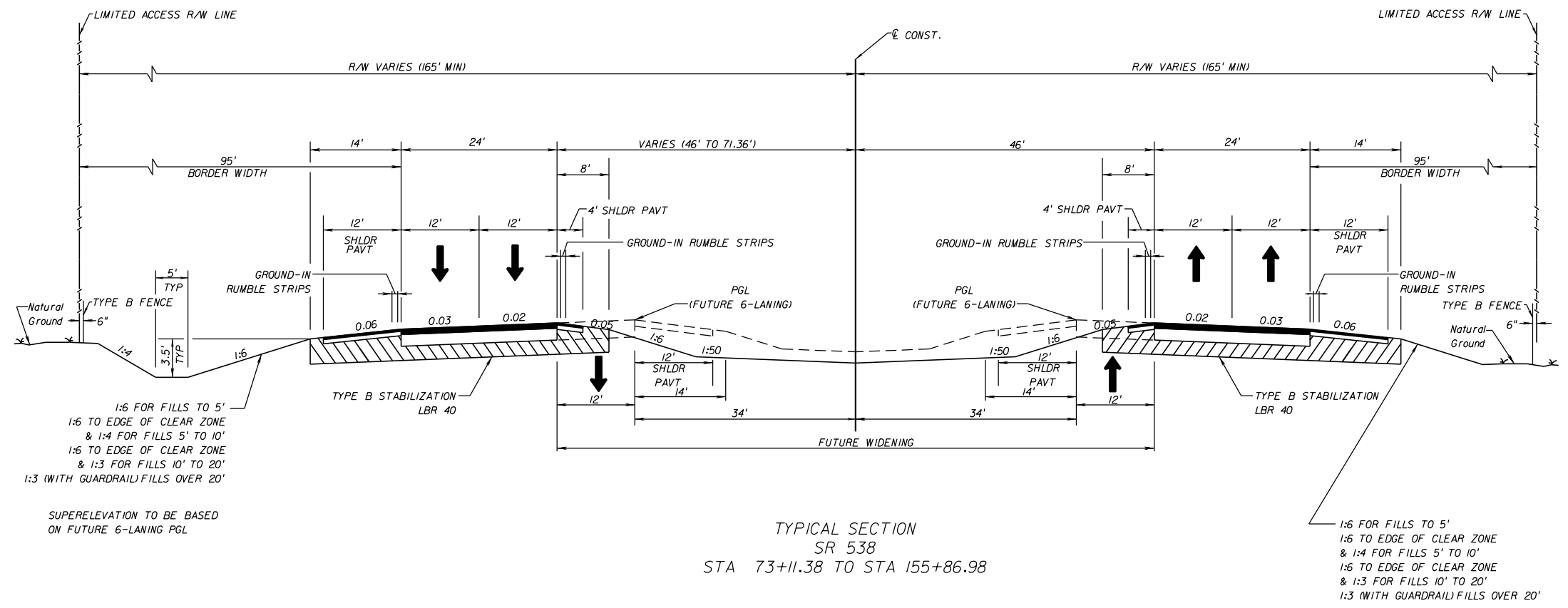
## ACCESS CLASSIFICATION

- (X) 1 - FREEWAY
- ( ) 2 - RESTRICTIVE W/Service Roads
- ( ) 3 - RESTRICTIVE W/660 ft. Connection Spacing
- ( ) 4 - NON-RESTRICTIVE W/2640 ft. Signal Spacing
- ( ) 5 - RESTRICTIVE W/440 ft. Connection Spacing
- ( ) 6 - NON-RESTRICTIVE W/1320 ft. Signal Spacing
- ( ) 7 - BOTH MEDIAN TYPES

## CRITERIA

- (X) NEW CONSTRUCTION / RECONSTRUCTION
- ( ) RESURFACING (LA FACILITIES)
- ( ) RRR (ARTERIALS & COLLECTORS)

## POTENTIAL EXCEPTIONS AND VARIATIONS RELATED TO TYPICAL SECTION:



## TRAFFIC DATA

CURRENT YEAR = 2019 AADT = N/A  
 ESTIMATED OPENING YEAR = 2025 AADT = 22,600  
 ESTIMATED DESIGN YEAR = 2045 AADT = 28,000  
 K = 11% D = 60% T (24 HOUR) = 6%  
 DESIGN SPEED = 70 MPH  
 POSTED SPEED = 65 MPH

SHEET NO.

3

THE OFFICIAL RECORD OF THIS SHEET IS THE ELECTRONIC FILE DIGITALLY SIGNED AND SEALED UNDER RULE 61G15-23.004, F.A.C.

# PROJECT CONTROLS

# TYPICAL SECTION NO.3 SR 538

## CONTEXT CLASSIFICATION

- ( ) C1 : NATURAL      ( ) C3C : SUBURBAN COMM.
- ( ) C2 : RURAL        ( ) C4 : URBAN GENERAL
- ( ) C2T : RURAL TOWN ( ) C5 : URBAN CENTER
- ( ) C3R : TOWN        ( ) C6 : URBAN CORE
- (X) N/A : LA FACILITY

## FUNCTIONAL CLASSIFICATION

- ( ) INTERSTATE        ( ) MAJOR COLL.
- (X) FREEWAY/EXPWY. ( ) MINOR COLL.
- ( ) PRINCIPAL ARTERIAL ( ) LOCAL
- ( ) MINOR ARTERIAL

## HIGHWAY SYSTEM

- ( ) NATIONAL HIGHWAY SYSTEM
- ( ) STRATEGIC INTERMODAL SYSTEM
- (X) STATE HIGHWAY SYSTEM
- ( ) OFF STATE HIGHWAY SYSTEM

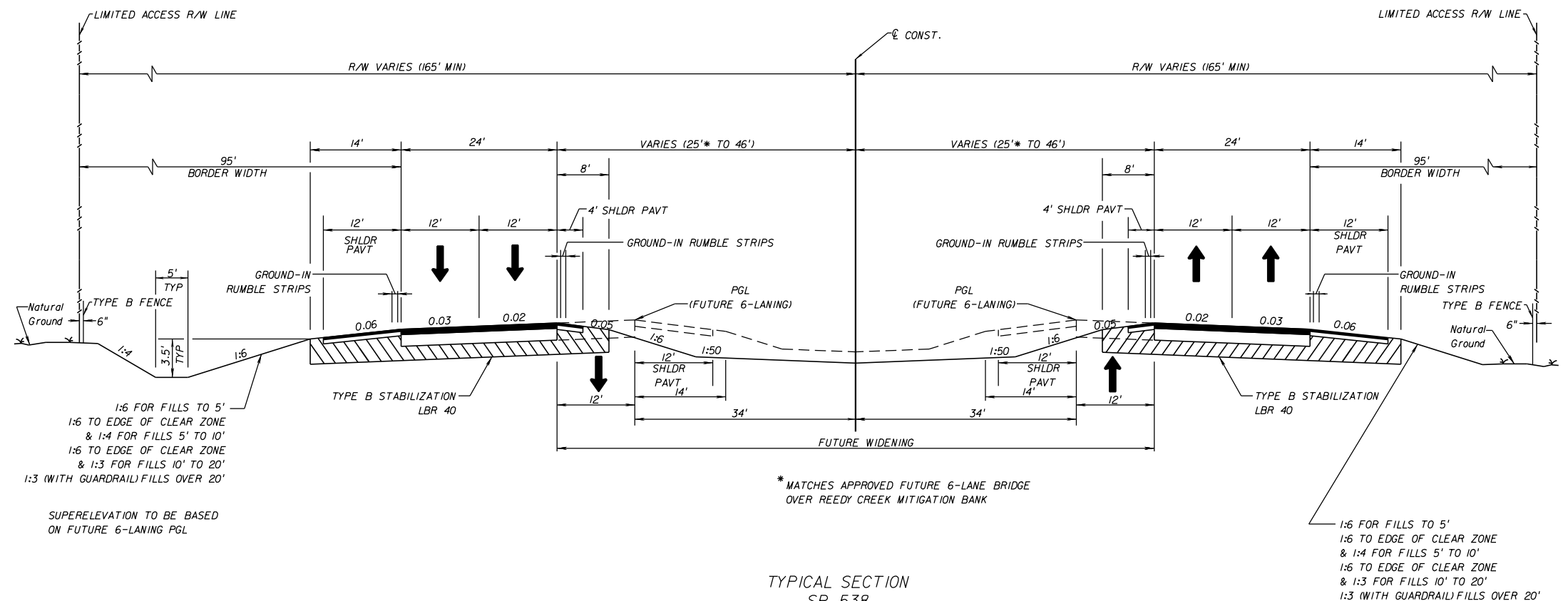
## ACCESS CLASSIFICATION

- (X) 1 - FREEWAY
- ( ) 2 - RESTRICTIVE W/Service Roads
- ( ) 3 - RESTRICTIVE W/660 ft. Connection Spacing
- ( ) 4 - NON-RESTRICTIVE W/2640 ft. Signal Spacing
- ( ) 5 - RESTRICTIVE W/440 ft. Connection Spacing
- ( ) 6 - NON-RESTRICTIVE W/1320 ft. Signal Spacing
- ( ) 7 - BOTH MEDIAN TYPES

## CRITERIA

- (X) NEW CONSTRUCTION / RECONSTRUCTION
- ( ) RESURFACING (LA FACILITIES)
- ( ) RRR (ARTERIALS & COLLECTORS)

## POTENTIAL EXCEPTIONS AND VARIATIONS RELATED TO TYPICAL SECTION:



## TRAFFIC DATA

CURRENT YEAR = 2019 AADT = N/A  
 ESTIMATED OPENING YEAR = 2025 AADT = 22,600  
 ESTIMATED DESIGN YEAR = 2045 AADT = 28,000  
 K = 11% D = 60% T (24 HOUR) = 6%  
 DESIGN SPEED = 70 MPH  
 POSTED SPEED = 65 MPH

SHEET NO.

4

# PROJECT CONTROLS

# TYPICAL SECTION NO. 4 SR 538

## CONTEXT CLASSIFICATION

- ( ) C1 : NATURAL      ( ) C3C : SUBURBAN COMM.
- ( ) C2 : RURAL        ( ) C4 : URBAN GENERAL
- ( ) C2T : RURAL TOWN   ( ) C5 : URBAN CENTER
- ( ) C3R : TOWN        ( ) C6 : URBAN CORE
- (X) N/A : LA FACILITY

## FUNCTIONAL CLASSIFICATION

- ( ) INTERSTATE        ( ) MAJOR COLL.
- (X) FREEWAY/EXPWY.   ( ) MINOR COLL.
- ( ) PRINCIPAL ARTERIAL   ( ) LOCAL
- ( ) MINOR ARTERIAL

## HIGHWAY SYSTEM

- ( ) NATIONAL HIGHWAY SYSTEM
- ( ) STRATEGIC INTERMODAL SYSTEM
- (X) STATE HIGHWAY SYSTEM
- ( ) OFF STATE HIGHWAY SYSTEM

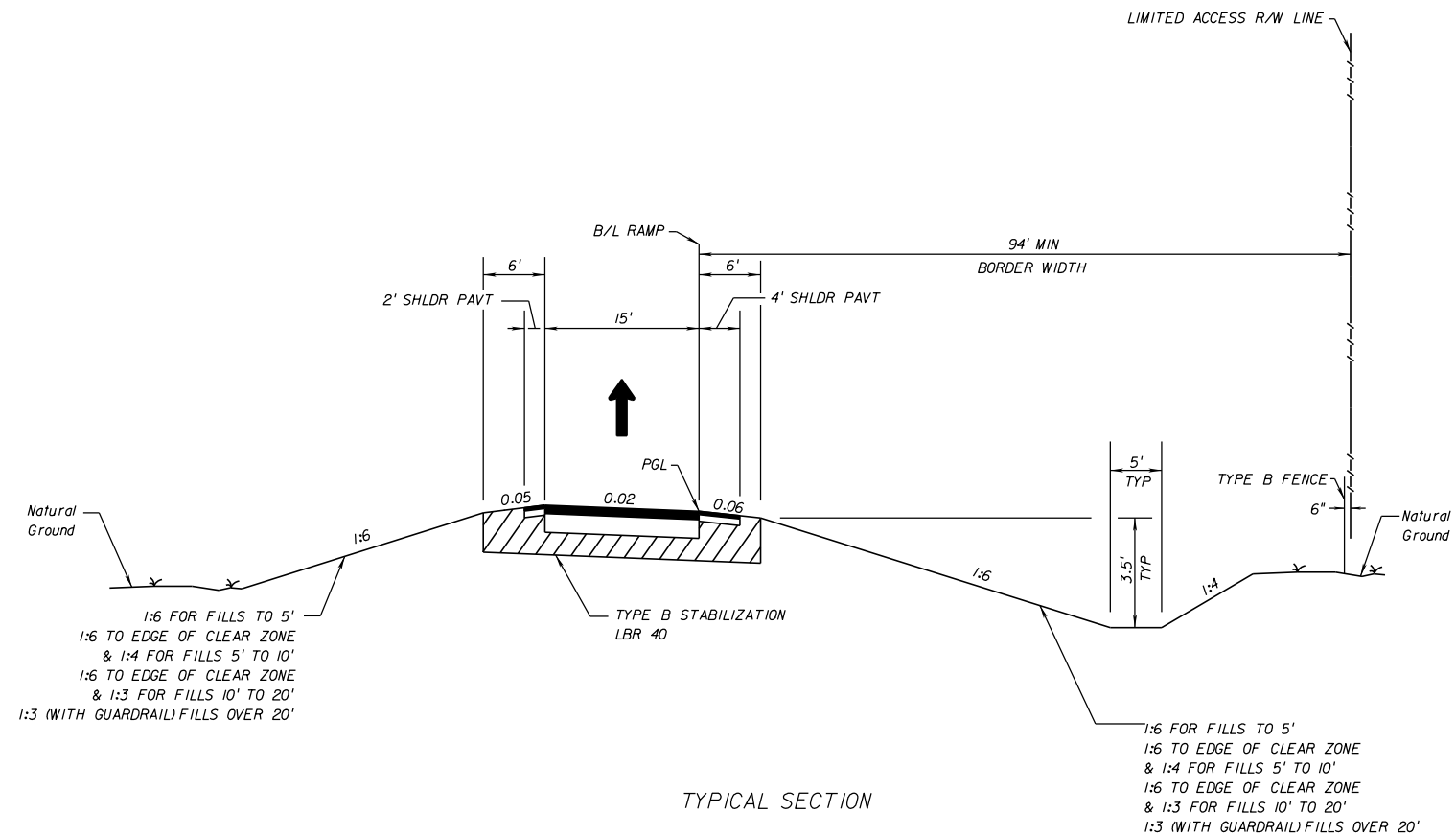
## ACCESS CLASSIFICATION

- (X) 1 - FREEWAY
- ( ) 2 - RESTRICTIVE W/Service Roads
- ( ) 3 - RESTRICTIVE W/660 ft. Connection Spacing
- ( ) 4 - NON-RESTRICTIVE W/2640 ft. Signal Spacing
- ( ) 5 - RESTRICTIVE W/440 ft. Connection Spacing
- ( ) 6 - NON-RESTRICTIVE W/1320 ft. Signal Spacing
- ( ) 7 - BOTH MEDIAN TYPES

## CRITERIA

- (X) NEW CONSTRUCTION / RECONSTRUCTION
- ( ) RESURFACING (LA FACILITIES)
- ( ) RRR (ARTERIALS & COLLECTORS)

## POTENTIAL EXCEPTIONS AND VARIATIONS RELATED TO TYPICAL SECTION:



- RAMP A1 - (WB OFF RAMP TO EB CR 432)
- RAMP A2A - (WB ON RAMP FROM WB SR 600)
- RAMP B1 - (EB OFF RAMP TO SR 600)
- RAMP C1 - (EB ON RAMP FROM EB SR 600)
- RAMP D1A - (WB OFF RAMP TO EB SR 600)
- RAMP D2 - (WB OFF RAMP TO SR 600)
- RAMP E1 - (EB ON RAMP FROM EB CR 532)
- RAMP E2 - (EB ON RAMP FROM WB CR 532)

## TRAFFIC DATA

CURRENT YEAR = 2019      AADT = N/A  
 ESTIMATED OPENING YEAR = 2025      AADT = TBD  
 ESTIMATED DESIGN YEAR = 2045      AADT = TBD  
 K = TBD    D = TBD    T (24 HOUR) = TBD  
 DESIGN SPEED = 50 MPH  
 POSTED SPEED = 50 MPH

SHEET NO.

5

# PROJECT CONTROLS

# TYPICAL SECTION NO.5 SR 538

## CONTEXT CLASSIFICATION

- ( ) C1 : NATURAL      ( ) C3C : SUBURBAN COMM.
- ( ) C2 : RURAL        ( ) C4 : URBAN GENERAL
- ( ) C2T : RURAL TOWN    ( ) C5 : URBAN CENTER
- ( ) C3R : TOWN        ( ) C6 : URBAN CORE
- (X) N/A : LA FACILITY

## FUNCTIONAL CLASSIFICATION

- ( ) INTERSTATE        ( ) MAJOR COLL.
- (X) FREEWAY/EXPWY.    ( ) MINOR COLL.
- ( ) PRINCIPAL ARTERIAL    ( ) LOCAL
- ( ) MINOR ARTERIAL

## HIGHWAY SYSTEM

- ( ) NATIONAL HIGHWAY SYSTEM
- ( ) STRATEGIC INTERMODAL SYSTEM
- (X) STATE HIGHWAY SYSTEM
- ( ) OFF STATE HIGHWAY SYSTEM

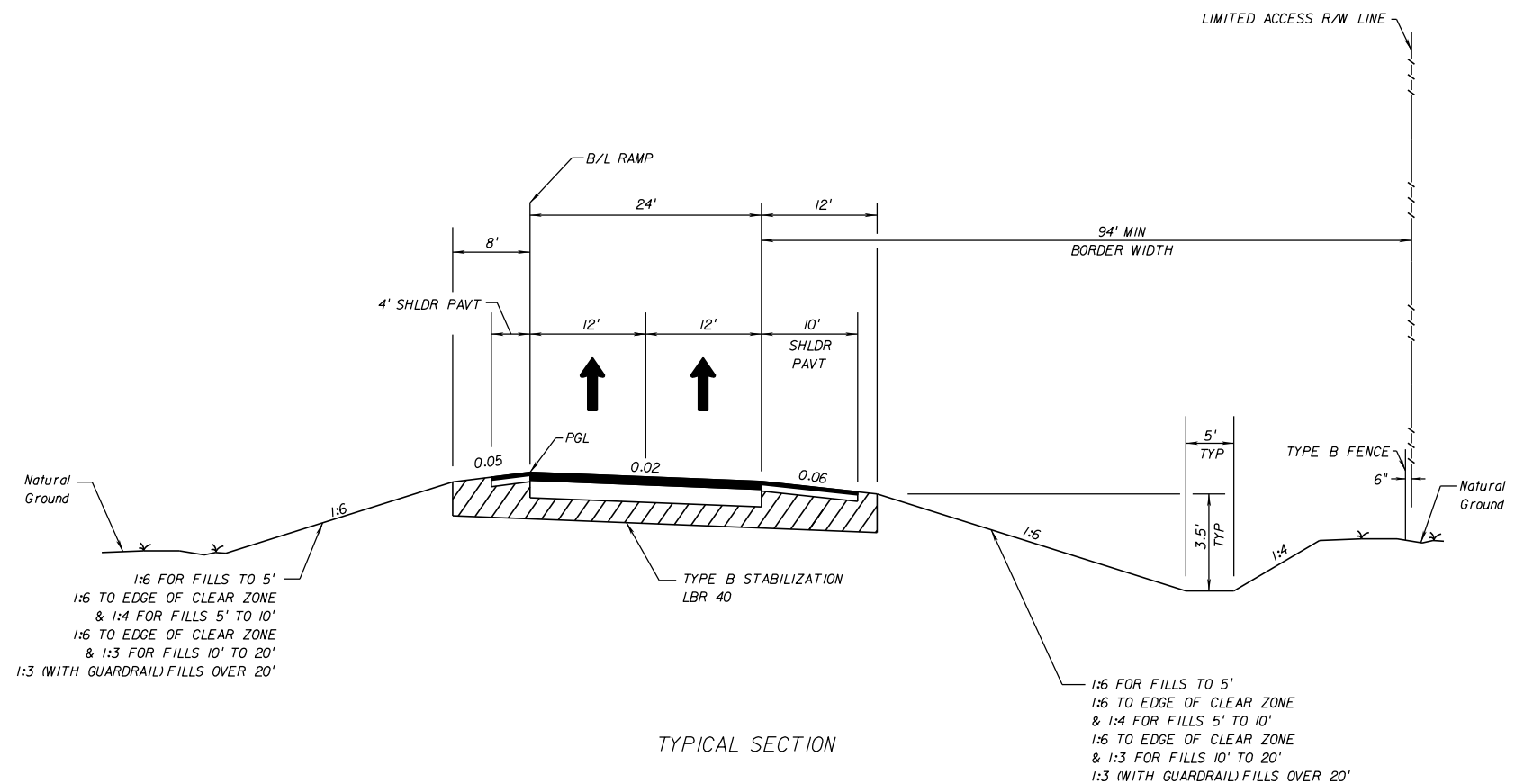
## ACCESS CLASSIFICATION

- (X) 1 - FREEWAY
- ( ) 2 - RESTRICTIVE W/Service Roads
- ( ) 3 - RESTRICTIVE W/660 ft. Connection Spacing
- ( ) 4 - NON-RESTRICTIVE W/2640 ft. Signal Spacing
- ( ) 5 - RESTRICTIVE W/440 ft. Connection Spacing
- ( ) 6 - NON-RESTRICTIVE W/1320 ft. Signal Spacing
- ( ) 7 - BOTH MEDIAN TYPES

## CRITERIA

- (X) NEW CONSTRUCTION / RECONSTRUCTION
- ( ) RESURFACING (LA FACILITIES)
- ( ) RRR (ARTERIALS & COLLECTORS)

## POTENTIAL EXCEPTIONS AND VARIATIONS RELATED TO TYPICAL SECTION:



TYPICAL SECTION  
 RAMP A2 - (WB ON RAMP FROM EB SR 600)  
 RAMP C2 - (EB ON RAMP FROM WB SR 600)  
 RAMP DI - (WB OFF RAMP TO WB SR 600)

## TRAFFIC DATA

CURRENT YEAR = 2019    AADT = N/A  
 ESTIMATED OPENING YEAR = 2025    AADT = TBD  
 ESTIMATED DESIGN YEAR = 2045    AADT = TBD  
 K = TBD    D = TBD    T (24 HOUR) = TBD  
 DESIGN SPEED = 50 MPH  
 POSTED SPEED = 50 MPH

SHEET NO.

6

THE OFFICIAL RECORD OF THIS SHEET IS THE ELECTRONIC FILE DIGITALLY SIGNED AND SEALED UNDER RULE 61G15-23.004, F.A.C.

# PROJECT CONTROLS

# TYPICAL SECTION NO. 6 SR 538

## CONTEXT CLASSIFICATION

- ( ) C1 : NATURAL      ( ) C3C : SUBURBAN COMM.
- ( ) C2 : RURAL        ( ) C4 : URBAN GENERAL
- ( ) C2T : RURAL TOWN ( ) C5 : URBAN CENTER
- ( ) C3R : TOWN        ( ) C6 : URBAN CORE
- (X) N/A : LA FACILITY

## FUNCTIONAL CLASSIFICATION

- ( ) INTERSTATE        ( ) MAJOR COLL.
- (X) FREEWAY/EXPWY. ( ) MINOR COLL.
- ( ) PRINCIPAL ARTERIAL ( ) LOCAL
- ( ) MINOR ARTERIAL

## HIGHWAY SYSTEM

- ( ) NATIONAL HIGHWAY SYSTEM
- ( ) STRATEGIC INTERMODAL SYSTEM
- (X) STATE HIGHWAY SYSTEM
- ( ) OFF STATE HIGHWAY SYSTEM

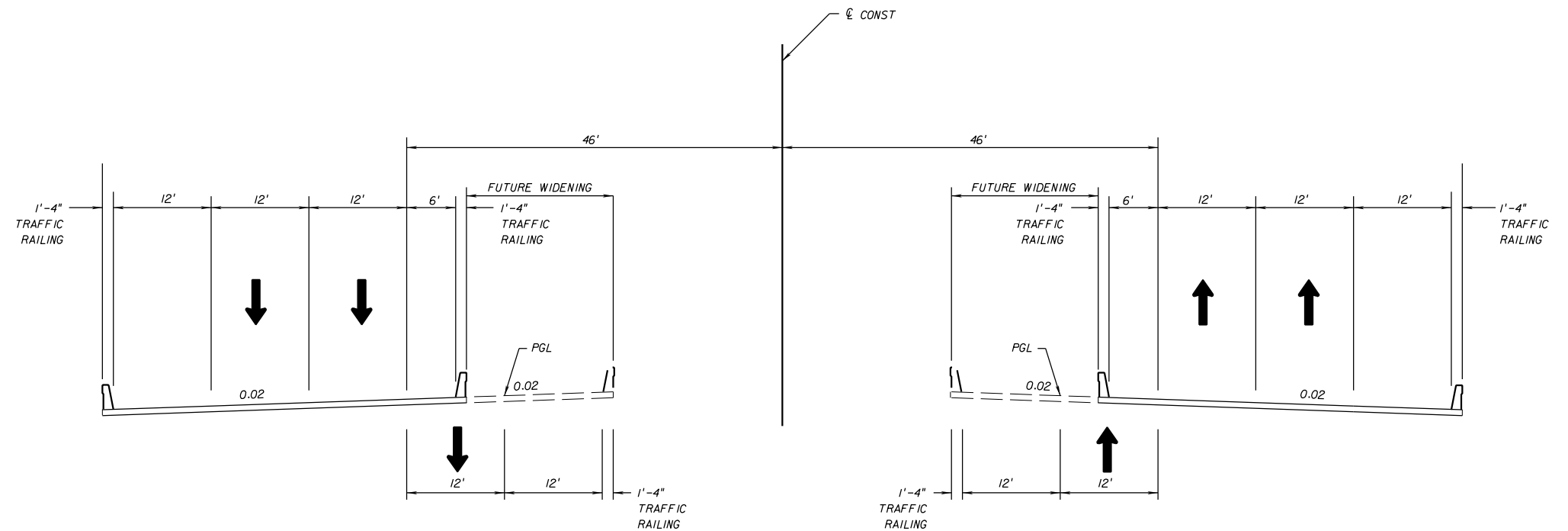
## ACCESS CLASSIFICATION

- (X) 1 - FREEWAY
- ( ) 2 - RESTRICTIVE W/Service Roads
- ( ) 3 - RESTRICTIVE W/660 ft. Connection Spacing
- ( ) 4 - NON-RESTRICTIVE W/2640 ft. Signal Spacing
- ( ) 5 - RESTRICTIVE W/440 ft. Connection Spacing
- ( ) 6 - NON-RESTRICTIVE W/1320 ft. Signal Spacing
- ( ) 7 - BOTH MEDIAN TYPES

## CRITERIA

- (X) NEW CONSTRUCTION / RECONSTRUCTION
- ( ) RESURFACING (LA FACILITIES)
- ( ) RRR (ARTERIALS & COLLECTORS)

## POTENTIAL EXCEPTIONS AND VARIATIONS RELATED TO TYPICAL SECTION:



BRIDGE TYPICAL SECTION

BRIDGE NO. 6 - (WB OVER OLD TAMPA HWY)  
BRIDGE NO. 9 - (WB OVER SR 600)  
BRIDGE NO. 11 - (WB OVER MITIGATION BANK)

BRIDGE TYPICAL SECTION

BRIDGE NO. 2 - (EB OVER CSX RR)  
BRIDGE NO. 5 - (EB OVER OLD TAMPA HWY)  
BRIDGE NO. 8 - (EB OVER SR 600)  
BRIDGE NO. 10 - (EB OVER MITIGATION BANK)

## TRAFFIC DATA

CURRENT YEAR = 2019 AADT = N/A  
ESTIMATED OPENING YEAR = 2025 AADT = 22,600  
ESTIMATED DESIGN YEAR = 2045 AADT = 28,000  
K = 11% D = 60% T (24 HOUR) = 6%  
DESIGN SPEED = 70 MPH  
POSTED SPEED = 65 MPH

SHEET NO.

7

THE OFFICIAL RECORD OF THIS SHEET IS THE ELECTRONIC FILE DIGITALLY SIGNED AND SEALED UNDER RULE 61G15-23.004, F.A.C.

**PROJECT CONTROLS**

**TYPICAL SECTION NO.7  
SR 538**

CONTEXT CLASSIFICATION

- ( ) C1 : NATURAL      ( ) C3C : SUBURBAN COMM.
- ( ) C2 : RURAL        ( ) C4 : URBAN GENERAL
- ( ) C2T : RURAL TOWN ( ) C5 : URBAN CENTER
- ( ) C3R : TOWN        ( ) C6 : URBAN CORE
- (X) N/A : LA FACILITY

FUNCTIONAL CLASSIFICATION

- ( ) INTERSTATE        ( ) MAJOR COLL.
- (X) FREEWAY/EXPWY. ( ) MINOR COLL.
- ( ) PRINCIPAL ARTERIAL ( ) LOCAL
- ( ) MINOR ARTERIAL

HIGHWAY SYSTEM

- ( ) NATIONAL HIGHWAY SYSTEM
- ( ) STRATEGIC INTERMODAL SYSTEM
- (X) STATE HIGHWAY SYSTEM
- ( ) OFF STATE HIGHWAY SYSTEM

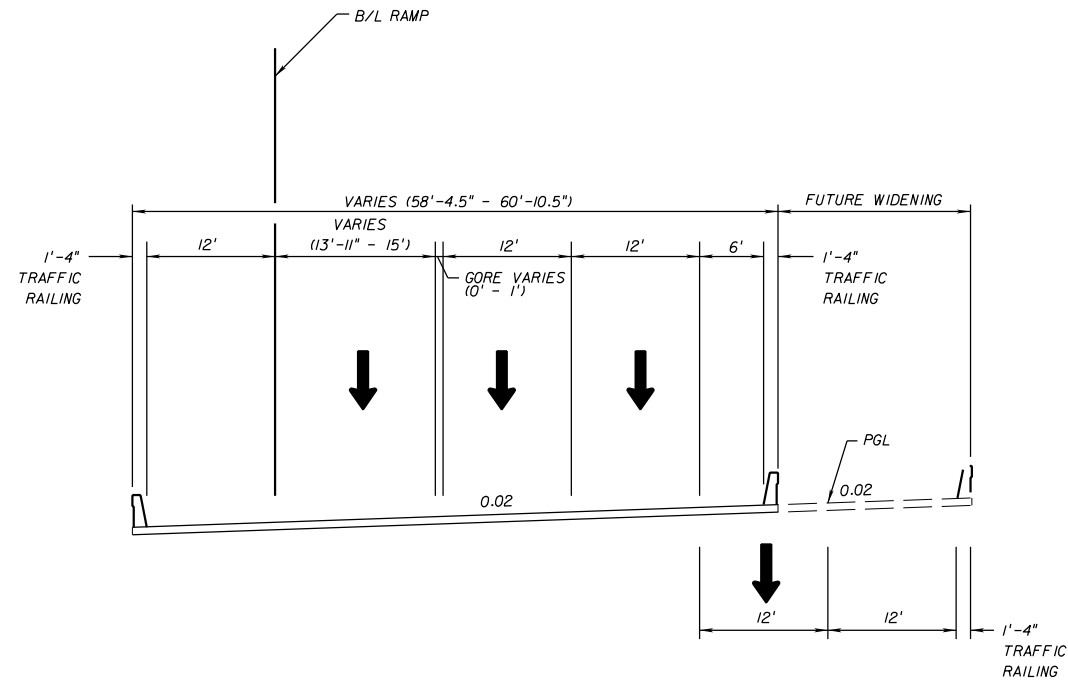
ACCESS CLASSIFICATION

- (X) 1 - FREEWAY
- ( ) 2 - RESTRICTIVE W/Service Roads
- ( ) 3 - RESTRICTIVE W/660 ft. Connection Spacing
- ( ) 4 - NON-RESTRICTIVE W/2640 ft. Signal Spacing
- ( ) 5 - RESTRICTIVE W/440 ft. Connection Spacing
- ( ) 6 - NON-RESTRICTIVE W/1320 ft. Signal Spacing
- ( ) 7 - BOTH MEDIAN TYPES

CRITERIA

- (X) NEW CONSTRUCTION / RECONSTRUCTION
- ( ) RESURFACING (LA FACILITIES)
- ( ) RRR (ARTERIALS & COLLECTORS)

POTENTIAL EXCEPTIONS AND VARIATIONS RELATED TO TYPICAL SECTION:



BRIDGE TYPICAL SECTION  
BRIDGE NO. 3 - (WB OVER CSX RR)

TRAFFIC DATA

CURRENT YEAR = 2019 AADT = N/A  
 ESTIMATED OPENING YEAR = 2025 AADT = 22,600  
 ESTIMATED DESIGN YEAR = 2045 AADT = 28,000  
 K = 11% D = 60% T (24 HOUR) = 6%  
 DESIGN SPEED = 70 MPH  
 POSTED SPEED = 65 MPH

SHEET NO.

8

THE OFFICIAL RECORD OF THIS SHEET IS THE ELECTRONIC FILE DIGITALLY SIGNED AND SEALED UNDER RULE 61G15-23.004, F.A.C.



**PROJECT CONTROLS**

**TYPICAL SECTION NO.8  
SR 538**

CONTEXT CLASSIFICATION

- ( ) C1 : NATURAL      ( ) C3C : SUBURBAN COMM.
- ( ) C2 : RURAL        ( ) C4 : URBAN GENERAL
- ( ) C2T : RURAL TOWN   ( ) C5 : URBAN CENTER
- ( ) C3R : TOWN        ( ) C6 : URBAN CORE
- (X) N/A : LA FACILITY

FUNCTIONAL CLASSIFICATION

- ( ) INTERSTATE        ( ) MAJOR COLL.
- (X) FREEWAY/EXPWY.   ( ) MINOR COLL.
- ( ) PRINCIPAL ARTERIAL   ( ) LOCAL
- ( ) MINOR ARTERIAL

HIGHWAY SYSTEM

- ( ) NATIONAL HIGHWAY SYSTEM
- ( ) STRATEGIC INTERMODAL SYSTEM
- (X) STATE HIGHWAY SYSTEM
- ( ) OFF STATE HIGHWAY SYSTEM

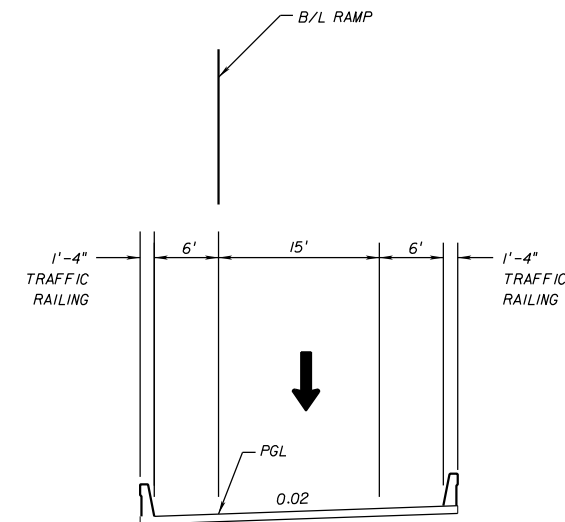
ACCESS CLASSIFICATION

- (X) 1 - FREEWAY
- ( ) 2 - RESTRICTIVE W/Service Roads
- ( ) 3 - RESTRICTIVE W/660 ft. Connection Spacing
- ( ) 4 - NON-RESTRICTIVE W/2640 ft. Signal Spacing
- ( ) 5 - RESTRICTIVE W/440 ft. Connection Spacing
- ( ) 6 - NON-RESTRICTIVE W/1320 ft. Signal Spacing
- ( ) 7 - BOTH MEDIAN TYPES

CRITERIA

- (X) NEW CONSTRUCTION / RECONSTRUCTION
- ( ) RESURFACING (LA FACILITIES)
- ( ) RRR (ARTERIALS & COLLECTORS)

POTENTIAL EXCEPTIONS AND VARIATIONS  
RELATED TO TYPICAL SECTION:



BRIDGE TYPICAL SECTION

- BRIDGE NO. 1 - (EB ON RAMP OVER CSX RR)
- BRIDGE NO. 4 - (EB ON RAMP OVER OLD TAMPA HWY)
- BRIDGE NO. 7 - (WB ON RAMP OVER OLD TAMPA HWY)

TRAFFIC DATA

CURRENT YEAR                    = 2019    AADT = N/A  
 ESTIMATED OPENING YEAR   = 2025    AADT = TBD  
 ESTIMATED DESIGN YEAR   = 2045    AADT = TBD  
 K = TBD    D = TBD    T (24 HOUR) = TBD  
 DESIGN SPEED   = 50 MPH  
 POSTED SPEED   = 50 MPH

SHEET  
NO.

9

THE OFFICIAL RECORD OF THIS SHEET IS THE ELECTRONIC FILE DIGITALLY SIGNED AND SEALED UNDER RULE 61G15-23.004, F.A.C.

# PROJECT CONTROLS

# TYPICAL SECTION NO.9 CR 532

## CONTEXT CLASSIFICATION

- ( ) C1 : NATURAL      ( ) C3C : SUBURBAN COMM.
- (X) C2 : RURAL        ( ) C4 : URBAN GENERAL
- ( ) C2T : RURAL TOWN    ( ) C5 : URBAN CENTER
- ( ) C3R : TOWN        ( ) C6 : URBAN CORE
- ( ) N/A : LA FACILITY

## FUNCTIONAL CLASSIFICATION

- ( ) INTERSTATE        ( ) MAJOR COLL.
- ( ) FREEWAY/EXPWY.    ( ) MINOR COLL.
- ( ) PRINCIPAL ARTERIAL    ( ) LOCAL
- (X) MINOR ARTERIAL

## HIGHWAY SYSTEM

- ( ) NATIONAL HIGHWAY SYSTEM
- ( ) STRATEGIC INTERMODAL SYSTEM
- ( ) STATE HIGHWAY SYSTEM
- (X) OFF STATE HIGHWAY SYSTEM

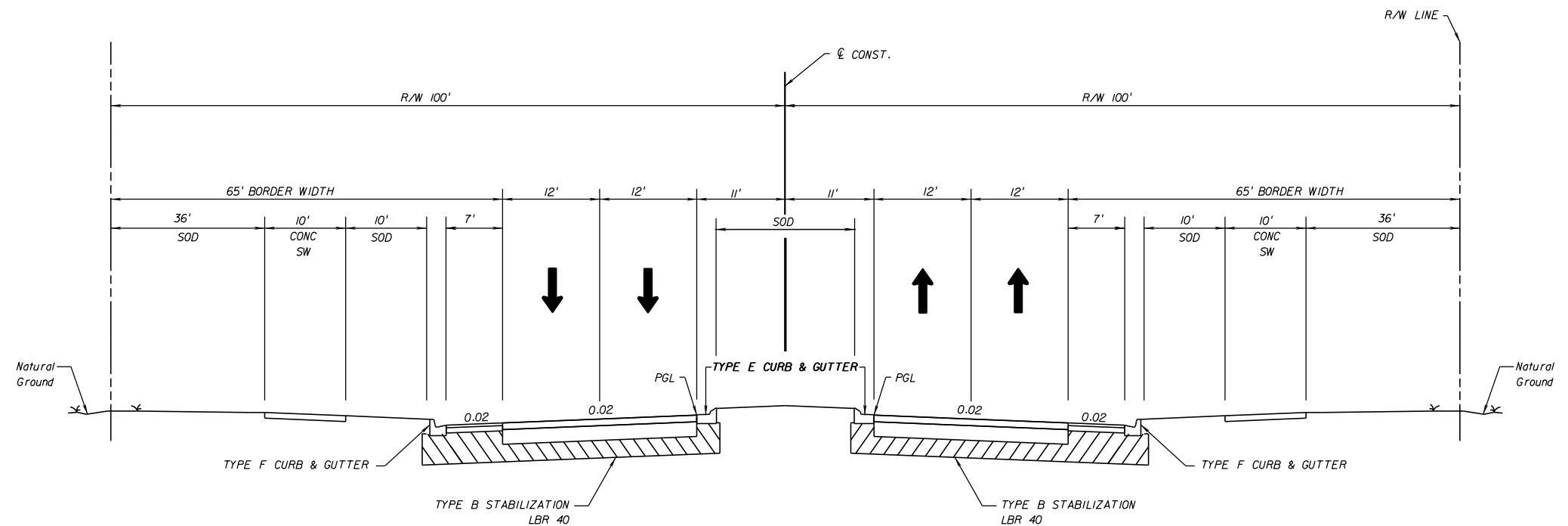
## ACCESS CLASSIFICATION

- ( ) 1 - FREEWAY
- ( ) 2 - RESTRICTIVE W/Service Roads
- ( ) 3 - RESTRICTIVE W/660 ft. Connection Spacing
- ( ) 4 - NON-RESTRICTIVE W/2640 ft. Signal Spacing
- (X) 5 - RESTRICTIVE W/440 ft. Connection Spacing
- ( ) 6 - NON-RESTRICTIVE W/1320 ft. Signal Spacing
- ( ) 7 - BOTH MEDIAN TYPES

## CRITERIA

- (X) NEW CONSTRUCTION / RECONSTRUCTION
- ( ) RESURFACING (LA FACILITIES)
- ( ) RRR (ARTERIALS & COLLECTORS)

## POTENTIAL EXCEPTIONS AND VARIATIONS RELATED TO TYPICAL SECTION:



TYPICAL SECTION  
CR 532

## TRAFFIC DATA

CURRENT YEAR = 2019 AADT = N/A  
 ESTIMATED OPENING YEAR = 2025 AADT = TBD  
 ESTIMATED DESIGN YEAR = 2045 AADT = TBD  
 K = TBD D = TBD T (24 HOUR) = TBD  
 DESIGN SPEED = 45 MPH  
 POSTED SPEED = 45 MPH

SHEET  
NO.

10

# PROJECT CONTROLS

# TYPICAL SECTION NO.10 SR 600 (US 17/92)

## CONTEXT CLASSIFICATION

- ( ) C1 : NATURAL      ( ) C3C : SUBURBAN COMM.
- (X) C2 : RURAL        ( ) C4 : URBAN GENERAL
- ( ) C2T : RURAL TOWN    ( ) C5 : URBAN CENTER
- ( ) C3R : TOWN        ( ) C6 : URBAN CORE
- ( ) N/A : LA FACILITY

## FUNCTIONAL CLASSIFICATION

- ( ) INTERSTATE        ( ) MAJOR COLL.
- ( ) FREEWAY/EXPWY.    ( ) MINOR COLL.
- (X) PRINCIPAL ARTERIAL    ( ) LOCAL
- ( ) MINOR ARTERIAL

## HIGHWAY SYSTEM

- (X) NATIONAL HIGHWAY SYSTEM
- ( ) STRATEGIC INTERMODAL SYSTEM
- (X) STATE HIGHWAY SYSTEM
- ( ) OFF STATE HIGHWAY SYSTEM

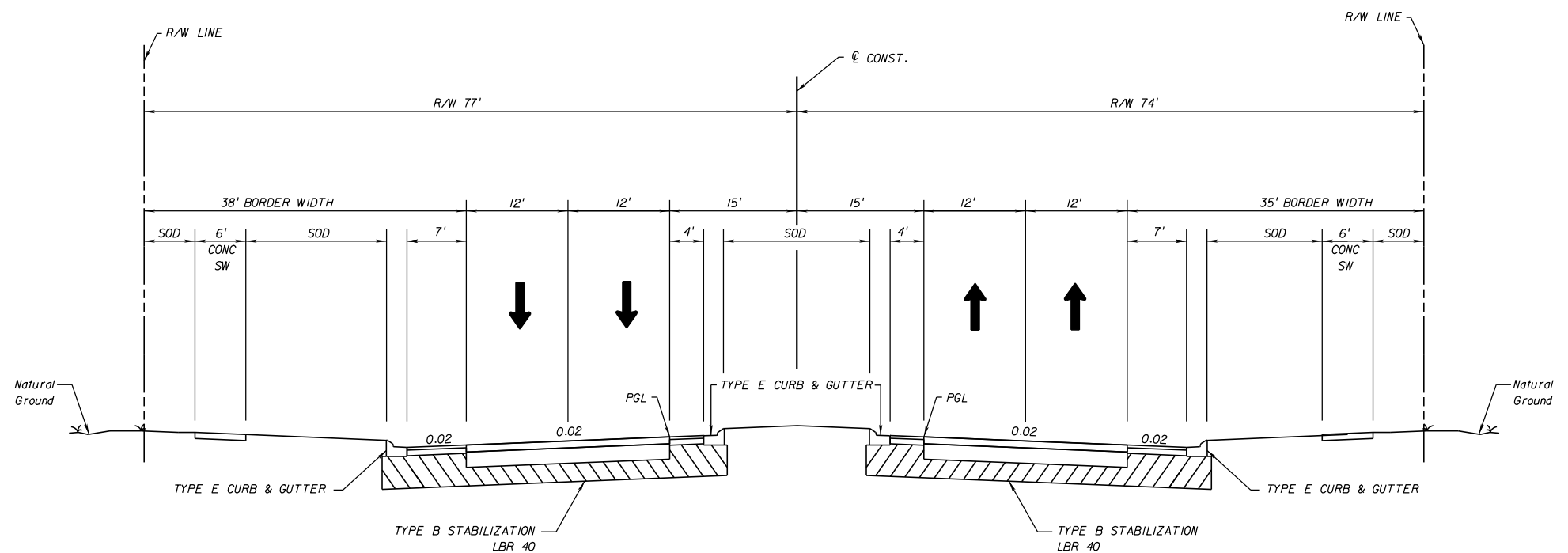
## ACCESS CLASSIFICATION

- ( ) 1 - FREEWAY
- ( ) 2 - RESTRICTIVE W/Service Roads
- (X) 3 - RESTRICTIVE W/660 ft. Connection Spacing
- ( ) 4 - NON-RESTRICTIVE W/2640 ft. Signal Spacing
- ( ) 5 - RESTRICTIVE W/440 ft. Connection Spacing
- ( ) 6 - NON-RESTRICTIVE W/1320 ft. Signal Spacing
- ( ) 7 - BOTH MEDIAN TYPES

## CRITERIA

- (X) NEW CONSTRUCTION / RECONSTRUCTION
- ( ) RESURFACING (LA FACILITIES)
- ( ) RRR (ARTERIALS & COLLECTORS)

## POTENTIAL EXCEPTIONS AND VARIATIONS RELATED TO TYPICAL SECTION:



TYPICAL SECTION  
SR 600 (US 17/92)

## TRAFFIC DATA

CURRENT YEAR = 2019 AADT = N/A  
 ESTIMATED OPENING YEAR = 2025 AADT = TBD  
 ESTIMATED DESIGN YEAR = 2045 AADT = TBD  
 K = TBD D = TBD T (24 HOUR) = TBD  
 DESIGN SPEED = 55 MPH  
 POSTED SPEED = 55 MPH

SHEET  
NO.

11