



2018

# Annual Inspection Report

*Volume I*





January 9, 2019

Mr. Joseph A. Berenis, P.E.  
Deputy Executive Director  
Central Florida Expressway Authority  
4974 ORL Tower Road  
Orlando, Florida 32807

Re: 2018 Annual Inspection Report

Dear Mr. Berenis:

In accordance with the requirements of Article V, Section 5.12 (C), of the Amended and Restated Master Bond Resolution, Dewberry is pleased to submit two (2) copies of the 2018 Annual Inspection Report. This report summarizes our observations based on a visual examination of the facilities that constitute the Central Florida Expressway Authority (CFX) system.

At the time the 2018 inspection was performed, portions of the CFX System were under construction and not inspected. Those portions are identified in the report.

Dewberry conducted the System inspection from July –October 2018 and reports that the CFX system has been maintained in good repair, working order and condition. This observation was based on a general visual inspection of the roadway, walls, bridges, and facilities. Results of the inspections are presented in greater detail within this report.

Staff anticipates continuing improvements in all roadway features for these roadways in the coming year as a result of the routine maintenance program and special projects. The observations that were noted can be evaluated and appropriate action taken by the CFX Maintenance Department.

The latest bridge inspection reports have been transmitted from the Florida Department of Transportation (FDOT) to the CFX. No load limits or weight restrictions have been imposed by FDOT that would prevent any bridge on the CFX system from being used. Currently 98.8% of CFX bridge structures have a condition rating of Good or higher.

Further, 92% of all CFX lane miles have a Pavement Condition Rating of Good or higher.

We have distributed copies of the report to the individuals listed below. Additional copies of the report are available should you need them.

We believe that this Annual Inspection Report continues to be an important tool in the identification of maintenance needs and planning asset management strategies. If you wish to discuss the report in more detail, please let us know.

Sincerely,



R. Keith Jackson, P.E.  
Program Manager

#### Attachments

cc: Don Budnovich (3 copies; includes copies for DBI & Jorgensen)  
Robert Glasemann (1 copy)  
Steve Geiss (1 copy)  
Brad Osterhaus (1 copy)  
Matt Lewis (1 copy)  
Chris Bloodwell (1 copy)  
Corey Quinn (1 copy)  
Glenn Pressimone (1 copy)  
Dave Wynne (1 copy)



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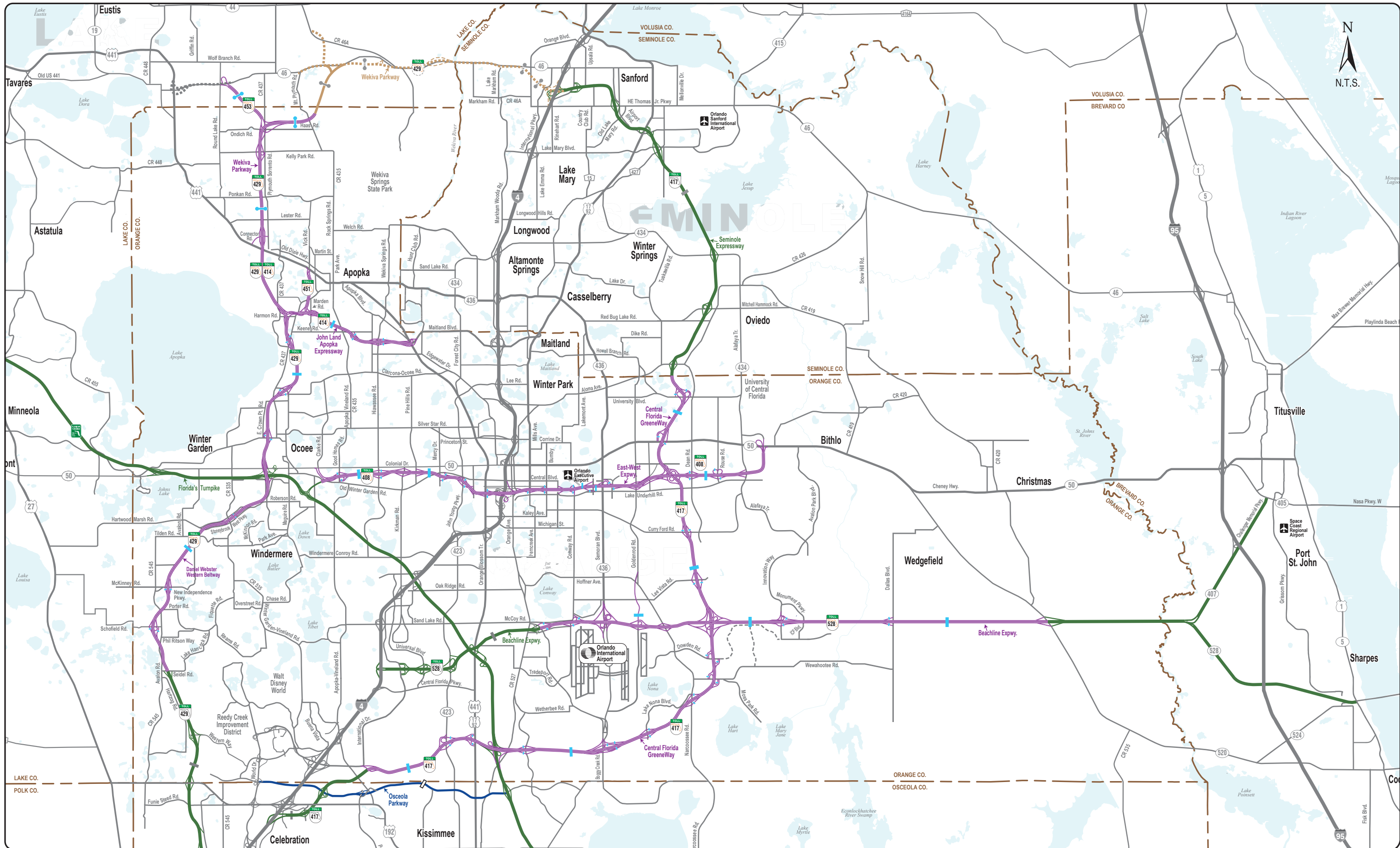
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# CFX SYSTEM WIDE MAP

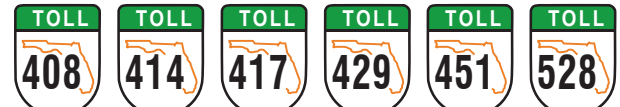
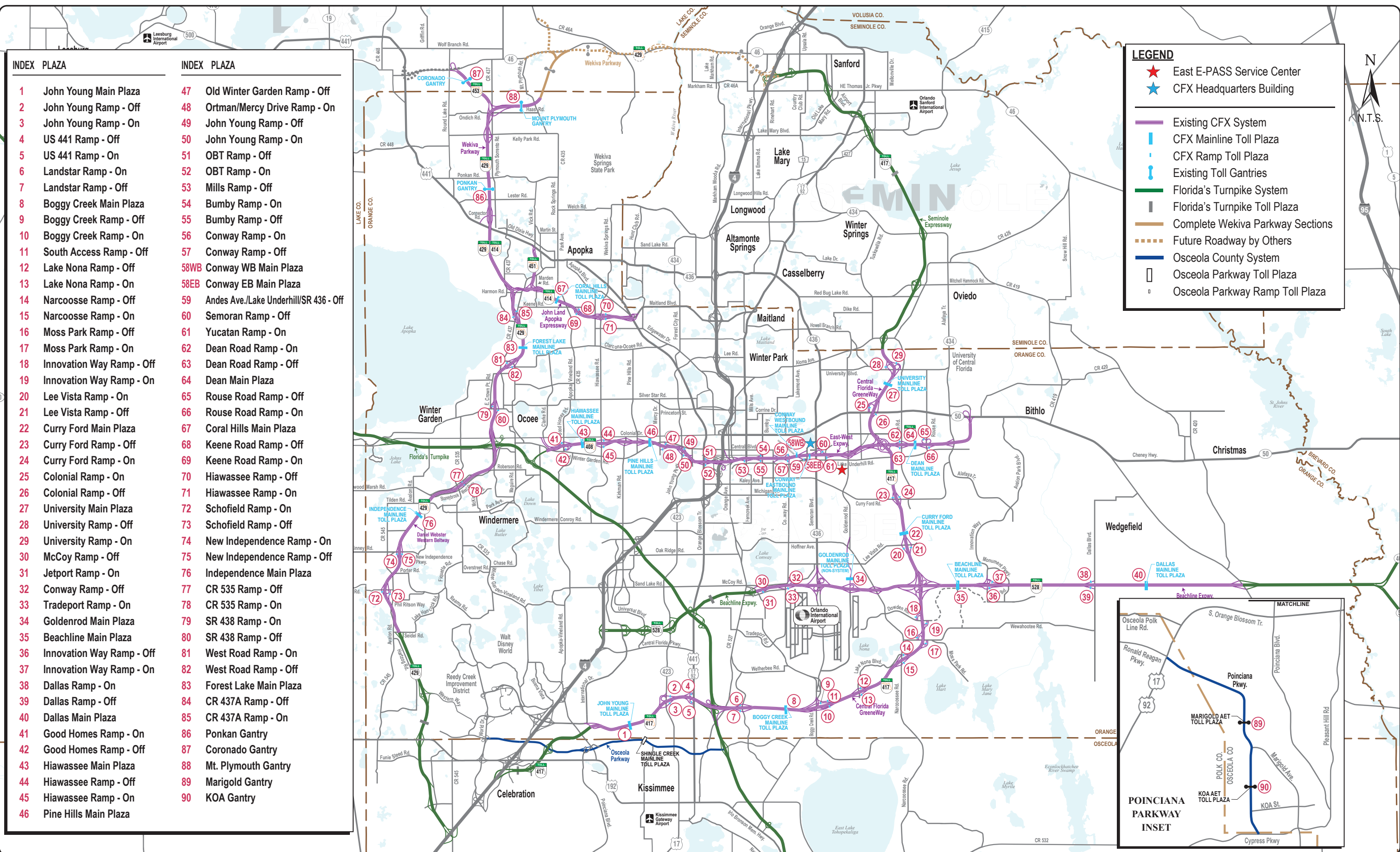


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# Executive Summary



2018 Annual Inspection Report  
CENTRAL FLORIDA EXPRESSWAY AUTHORITY



# Executive Summary

The 2018 inspection of the Central Florida Expressway Authority's (CFX) System, was performed by Dewberry, CFX's General Engineering Consultant, and included a visual inspection of the roadways, facilities, landscape, ponds, and bridges currently under CFX jurisdiction, with special emphasis on ponds and fence lines in 2018.

The CFX's roadway system includes SR 408 (East-West Expressway), SR 414 (John Land Apopka Expressway), SR 417 (Central Florida GreeneWay), SR 429 (Daniel Webster Western Beltway), SR 451, SR 453, and SR 528 (Beachline Expressway).

CFX also operates the Goldenrod Road Extension, a non-system two-mile tolled road with one mainline plaza.

Based on the findings for the 2018 inspection, it is concluded that the CFX roadway system has been well maintained and in good repair, working order, and condition. It is recommended that the Authority continue the implementation of routine maintenance as budgeted and scoped, and also continue to implement Renewal and Replacement projects as identified in the CFX's Five-Year Work Plan.





# Report



2018 Annual Inspection Report  
CENTRAL FLORIDA EXPRESSWAY AUTHORITY

# 1.0 Introduction

## 1.1 BACKGROUND

Between July and October 2018, Dewberry, the Central Florida Expressway Authority's (CFX) General Engineering Consultant (GEC), conducted the annual inspection of CFX's System as required by Article V, Section 5.12 (C) of the Amended and Restated Master Bond Resolution. These inspections provide a basis to plan funding levels needed to maintain assets for the Highway Maintenance Fund, Building Maintenance Fund, and Five – Year Work Plan Renewal and replacement projects budgets for the ensuing fiscal year (FY).

## 1.2 INSPECTION PROCESS

The GEC Annual Inspection process assessed four main elements: roadway, bridges, walls, and buildings / facilities. The inspection process excluded any part of the system that was currently under construction at the time of observations.

The roadway portion of the inspection focused on observations of ponds, fencing, pavements, drainage structures, erosion issues, signing, striping, illumination, barriers, main line and ramp plaza gantries, and overall safety of each corridor. Wall inspections focused on observations of panels, joint, coping, flumes, inlets, rails, slope paving, visible underdrain pipes, sound walls, and adjacent elements. Buildings / facilities observations focused on the interior and exteriors of mainline plazas, and ramp plazas. Based on current first and third floor construction taking place at the time of the inspection, the CFX Administration and Operations Center was not inspected.

Bridge and Sign Structures inspections were conducted by Certified Bridge and Structural Inspectors under a program administered by the Florida Department of Transportation (FDOT) and funded by CFX. Inspection results are published every two years on each bridge and sign structure. The bridge inspection process for the

basis of this report addressed items that could be visually observed based on FDOT bridge inspection reports.

Inspections involved a general visual examination of element features. The inspection results summarized in Section 2 reflect the condition of the feature(s) on the day the examination was performed. No detailed in-place or destructive testing was performed.

Items observed were recorded and rated using a three-point scale (Table 1):

**Table 1**

Rating	Action
3	Routine Maintenance Needed.
2	Poor Condition but Operable.
1	Requires Immediate Attention.

Many of the observations noted have already been corrected as part of the CFX's Assets Management Contracts.

During the 2018 inspection process, the following major construction projects were underway:

- SR 408 / 417 Interchange
- SR 408 / I-4 Interchange (FDOT Project)
- SR 408 Widening SR 417 to Alafaya Trail
- SR 408 Milling and Resurfacing, SR 50 to Westmoreland Drive
- SR 417 to SR 528 Ramp Re-alignment
- SR 417 Widening Econlockhatchee to Seminole County Line
- SR 417 / Turnpike Interchange (FDOT Project)
- SR 429 / CR 535 Ramp Improvements
- SR 528 Econlockhatchee River Bridge Replacement

As a result, some of these portions of the roadway were not inspected.

## 1.3 SYSTEM DESCRIPTION

CFX's roadway system consists of seven expressways: the Spessard L. Holland East-West Expressway and Arnold Palmer Expressway (SR 408), the John Land Apopka Expressway (SR 414), the Central Florida GreeneWay (SR 417), the Daniel Webster Western Beltway and Wekiva Parkway (SR 429), the Western Beltway Connector (SR 451), SR 453 and the Martin Andersen Beachline Expressway (SR 528). (Figure 1).

The system consists of 118 centerline miles of highway with 66 interchanges, 13 mainline toll plazas, 3 mainline gantries, 71 ramp toll plazas, 3 ramp gantries, and 335 bridges. CFX also operates a non-system, two-mile tolled expressway (Goldenrod Road Extension) with one mainline toll plaza. The inspection results for the Goldenrod Road Extension are included as non-system facility.

### 1.3.1 SR 408 Spessard L. Holland East-West Expressway

State Road 408 (Spessard L. Holland East-West Expressway) is a 22-mile toll road running east-west. This toll road connects Ocoee from Florida's Turnpike in west Orange County to SR 50 (Colonial Drive) east of Alafaya Trail near the University of Central Florida in east Orange County with the Florida's Turnpike owning, operating, and maintaining the portion from the Turnpike east to the Old Winter Garden Road Overpass.

### 1.3.2 SR 414 John Land Apopka Expressway

The first phase of State Road 414 (John Land Apopka Expressway) opened on May 15, 2009. SR 414 extends south and east from U.S. Highway 441 in Apopka to Maitland Boulevard. Of the 9 miles, 3 miles are part of a dual route with State Road 429 (Daniel Webster Western Beltway).

### 1.3.3 SR 417 Central Florida GreeneWay

State Road 417 (Central Florida GreeneWay), a 55-mile toll road operated by CFX and Florida's

Turnpike Enterprise. CFX operates the 33 miles of SR 417 in Orange County. Known as the Central Florida GreeneWay, this stretch runs from SR 535 / 536 (International Drive) to SR 426 (Aloma Avenue) at the Orange-Seminole County line.

### 1.3.4 SR 429 Daniel Webster Western Beltway / Wekiva Parkway

The Daniel Webster Western Beltway and Wekiva Parkway (SR 429) extend north from I-4 in Osceola County to SR 46 in Lake County. CFX's portion includes 31 miles from Seidel Road to just south of the Orange / Lake County line. Of the total 31 miles, 3 miles are part of the dual route with SR 414 (SR 429 / 414).

The portions of SR 429 from south of Seidel Road to I-4 and north of the Orange / Lake County line are owned and operated by the FTE and FDOT.

### 1.3.5 SR 451

Originally part of State Road 429 (Daniel Webster Western Beltway), SR 451 connects SR 414 (John Land Apopka Expressway) and SR 429 north to U.S. Highway 441 (SR 500 / Orange Blossom Trail at Vick Road). It is approximately 3 miles in length.

### 1.3.6 SR 453

State Road 453 is a tolled spur road connecting SR 429 (Wekiva Parkway) with State Road 46 east of Round Lake Road in Lake County. The Central Florida Expressway Authority built the 1.6-mile Mount Dora Connector as part of its overall contribution to the parkway project

### 1.3.7 SR 528 Martin Andersen Beachline Expressway

State Road 528 (Martin B. Anderson Beachline Expressway) was the first road on the expressway system. The approximately 52-mile expressway extends east from Interstate 4 across SR 417 (Central Florida GreeneWay) and Interstate 95, ending at U.S. Highway 1 in Cocoa.



The Beachline is owned and maintained by two agencies: CFX operating the 23 miles extending from Boggy Creek Road to SR 520, and the Florida's Turnpike Enterprise operating the remainder.

#### 1.3.8 CFX Facilities / Buildings

CFX operates facilities in support of the safe and reliable operation of the CFX's mobility network. These facilities include 13 mainline plazas, 71 ramp plazas, three mainline gantries, one back-up data center, and one administration and operations center (Figure 2).

CFX also owns and leases two facilities at 7001 McCoy Road and 525 S Magnolia Avenue. These facilities were not inspected.

### 1.4 CFX MAINTENANCE PROGRAM

The Maintenance Department for CFX is integral in providing the region with a world-class integrated mobility network. The Maintenance Department is responsible for day to day routine roadway and landscape maintenance for the System, certain System Renewal and Replacement projects, and facilities.

Maintenance Projects include, but are not limited to, repairs and maintenance, coatings, renewals, replacements, improvements, and other projects necessary for the safe and efficient operation of the System and to prevent loss of revenue. These projects include such costs for engineering, equipment purchases / additions and replacements, maintenance expenses for roadway, bridge, buildings, walls, etc. not occurring at annual or shorter periods.

The Maintenance Department utilizes outsourced resources to accomplish the requirements of routine maintenance. CFX has created contracts to provide these services to improve efficiency and to increase fiscal responsibility.

CFX employs two contractors to perform asset maintenance management services for roadways and bridges. Roy Jorgensen

Associates Inc. maintains SR 408, SR 417 and SR 528, while DBI maintains SR414, SR 429, and SR 451. Facilities are maintained by DBI. Each of these have a contract duration of five years with five renewal options of one year each.

Systemwide Landscape Maintenance is performed by two systemwide landscape maintenance contractors Commercial Companies, Inc. and Groundtek.

The Maintenance Department staff is supported by the GEC, Dewberry. As the GEC, Dewberry provides professional services in support of Maintenance Department responsibilities, which include items such as:

- Annual independent inspection and report concerning system condition
- Systemwide specific maintenance problems resolution
- CFX Pavement Management Program support
- Systemwide sign inventory and engineering for maintenance and replacement of signs
- Asset management needs analysis
- Identification of appropriate maintenance and repair actions and cycles to minimize deteriorating conditions of CFX assets.
- Environmental support

In addition the GEC, provides resources to support CFX management and administration of engineering activities associate with major maintenance projects. The disciplines Dewberry utilizes as the GEC include: civil, structural, traffic, environmental, mechanical, electrical engineering, and architectural services.

As part of the report preparation process, Dewberry met with CFX's Director of Maintenance to discuss the major achievements of the past fiscal year (FY 2017) and the goals and objectives for the coming fiscal year (FY 2019).



The following are identified as major achievements during the past fiscal year (FY 2019):

- Continued a landscape maintenance program to keep the aesthetics of the system at the desired levels.
- Continued a modified aquatic maintenance program to make stormwater ponds an aesthetic feature and part of the roadway landscape.
- Identified toll plaza roofs that have reached their expected useful life and programmed accordingly via the Five-Year Work Plan.
- Performed pavement inspections which identified areas that are raveling and / or damaged and required maintenance. These inspections identified repairs needed to extend the pavement life in order to meet the planned and programmed milling and resurfacing schedules based on the Five-Year Work Plan and the Pavement Management Program.
- Identified and inventoried systemwide steel bridge coatings to identify and program future coatings replacement cycles for the Five-Year Work Plan.
- Implemented the first phase of systemwide aesthetic coatings renewal projects.

CFX Maintenance Staff has identified the following goals and objectives for the coming fiscal year (FY 2019). These items involve activities associated with construction and maintenance of roadways, bridges, facilities, and landscape.

- Achieve a MRP score of at least 90.
- Continue to implement systemwide signing upgrades and rehabilitation.
- Continue the identification of projects that should be funded under the Renewal and Replacement budget.

- Continue the identification of projects that require routine maintenance and should be included in the maintenance budget.
- Maintain pavement striping at a high level of reflectivity by identifying areas in need of rehabilitation.
- Proceed with design for the next phases of the systemwide coatings upgrades.

#### **1.4.1 Roadway Maintenance Budget**

CFX's fiscal year (FY) 2019 Roadway Maintenance Fund, is budgeted at \$19.9 million for maintenance administration, routine maintenance, landscape maintenance, and pond maintenance.

#### **1.4.2 Facilities Maintenance Budget**

CFX toll facilities, as well as CFX's headquarters building, have a CFX's facilities maintenance budget for FY 2019 is approximately \$1.96 million.

#### **1.4.3 Systemwide Landscape and Aquatic Maintenance Budget**

CFX's fiscal year (FY) 2019 Systemwide Landscape Maintenance Fund is budgeted at \$4.0 million for maintenance administration and routine maintenance.

#### **1.4.4 Maintenance Rating Program**

CFX has instituted a Maintenance Rating Program (MRP) to evaluate the performance of roadway and bridge maintenance contractors. The MRP monitors current operations and is used to identify recurring problems. The program allows for early identification of maintenance issues, increase accountability, and provides assurance that assets are being maintained at an adequate level.

Under the MRP, sample units for different asset groups (roads, bridges, and facilities) are randomly selected for the entire year. Inspections are conducted every two months on a portion of the sample units from each corridor. Individual characteristics are evaluated on Pass / Fail criteria. The resulting scores are weighted and

combined for the asset groups. A total composite score is used to evaluate maintenance effectiveness.

#### 1.4.5 Specialized Inspection

Inspections of the bridges on CFX's system are conducted by Certified Bridge Inspectors under a program administered by the FDOT and funded by CFX. Detailed Bridge Inspection Reports are prepared and filed every two years for each bridge.

The most current bridge ratings were received from FDOT in August 2018. The Performance Rating rates the condition of the bridge. It is auto-calculated and is based on the lowest rating for Deck, Superstructure and Substructure. For culverts, the rating is based on the Culvert Rating. The performance rating factors are as follows:

- 1 = Excellent
- 2 = Good
- 3 = Fair
- 4 (and above) = Poor

There are currently 335 bridge structures on the system with 98.8% of them having a condition rating of Good or higher. There are currently no bridge condition weight restrictions. See Appendix 1 for a summary of FDOT Bridge Ratings.

CFX currently conducts a Pavement Management Analysis based on FDOT criteria. This report is used to assess the condition and performance of a roadway as well as predict future rehabilitation needs. The 2018 Pavement Condition Survey data shows 92% of CFX maintained lane miles currently have good or higher Pavement Condition Rating.

Measures goal of 85%. CFX continues to invest in the system's assets to deliver a world class mobility network

## 2.0 Inspection Findings

The inspection results summarized in this section reflect features that were rated two or three. Features with a rating of one were immediately reported to CFX maintenance staff for immediate maintenance.

### 2.1 SR 408 SPESSARD L. HOLLAND EAST-WEST EXPRESSWAY FINDINGS (RATINGS 2 OR 3).

See Appendix 3 for detailed inspection sheets, maps, and photos.

#### 2.1.1 SR 408 Spessard L. Holland East-West Expressway Roadway

The majority of observations were located on the more senior sections of SR 408 from the west end at Clarke Road to John Young Parkway. The observations included guardrail damage, erosion, drainage structures, overgrown slopes, and pavement. Roadside erosion was prevalent from Clarke Road to Pine Hills Road, in particular washouts creating drop off hazards. Pond slope erosion could be seen at various locations throughout the corridor.



**Figure 3** Roadside erosion east of Pine Hills Road.

### 2.1.2 SR 408 Spessard L. Holland East-West Expressway Bridges

Bridge observations included expansion joints with spalls and adhesion loss, bridge barrier coatings degradation, and graffiti.

Expansion joints exhibiting spalls and adhesion loss were present at Kirkman Road. Graffiti was observed on the pedestrian railing at Dean Road. Rusting connections for fencing and screening were observed at Dean Road.



**Figure 4** Graffiti on SR 408 Dean Road Bridge

### 2.1.3 SR 408 Spessard L. Holland East-West Expressway Walls and Fencing

Notable wall and fence observations included graffiti, color fade, vegetation overgrowing fence lines, and damaged or downed right-of-way (ROW) fencings.

Retaining walls with graffiti were observed at various locations throughout SR 408, including the SR 408 MSE walls at Chickasaw Trail.

Overgrown fence observations for both ROW and retaining wall were noted at various locations including, SR 50 / Colonial and Woodbury Road.



**Figure 5** Overgrown ROW fence line SR 408 near Woodbury Road.

### 2.1.4 SR 408 Spessard L. Holland East-West Expressway Facilities

SR 408 consists of 5 Mainline Toll Plazas (Hiawassee, Pine Hills, Conway West, Conway East, and Dean), and 23 ramp plazas.

The facilities along SR 408 are generally in good condition. Observations for various features in the facilities along SR 408 were typically for wiring, switchboards and breakers, receptacles, ceiling tiles and vents, duct work, and flooring.

Individual summaries of facilities along SR 408 are indicated in the Annual Facilities Inspection Reports included in this document.

## 2.2 SR 414 JOHN LAND APOPKA EXPRESSWAY FINDINGS (RATINGS 2 OR 3).

See Appendix 4 for detailed inspection sheets, maps, and photos.

### 2.2.1 SR 414 John Land Apopka Expressway Roadway

Roadway observations included erosion, drainage structures, and striping and pavement markings.

Roadside erosion was observed at the Keene Road off-ramp and the US 441 loop ramp.

Various drainage structures with varying degrees of obstruction and washout were observed along SR 414 including the pond



behind the Coral Hills Mainline Toll Plaza and the concrete flume west of Lakeville Road.

Missing and deteriorated pavement markings were observed throughout the roadway without a concentration in any specific area.



**Figure 6** Obstructed drainage / flume structure at SR 414 south of Lakeville Road.

### 2.2.2 SR 414 John Land Apopka Expressway Bridges

Bridge observations included spalled delaminated abutment joints, graffiti, ponding water, and delaminated parapets.

Abutment joint observations, graffiti and delamination were seen at Marden Road.

Ponding water was observed on the south side of the SR 414 bridge over CR 435. Ponding water was limited to the area adjacent to the traffic railing.



**Figure 7** Ponding water on SR 414 Bridge over CR 435.

### 2.2.3 SR 414 John Land Apopka Expressway Walls and Fencing

Notable fence observations included inoperable or missing gate locks at SR 441, damaged and or vegetation covered ROW fence along the corridor.



**Figure 8** Damaged ROW fence SR 414 bridge abutment (later repaired).

### 2.2.4 SR 414 John Land Apopka Expressway Facilities

SR 414 consists of 1 Mainline Toll Plaza (Coral Hills), and 5 ramp plazas.

The facilities along SR 414 were generally in good condition. Observations for various features in the facilities along SR 414 were typically for wiring, switchboards and breakers, receptacles, duct work, ceiling tiles, vents, and flooring. Individual summaries of facilities along SR 414 are indicated in the Annual Facilities Inspection Reports included in this document.



**Figure 9** Damaged ceiling tile Coral Hills Mainline IT room.



## 2.3 SR 417 CENTRAL FLORIDA GREENEWAY FINDINGS (RATINGS 2 OR 3).

See Appendix 5 for detailed inspection sheets, maps, and photos.

### 2.3.1 SR 417 Central Florida GreeneWay Roadway

Roadway observations included pavement edge ravel, drainage structure obstructions both sediment and vegetation, and roadside erosion.

Overgrown and damage fence observations were made at the Boggy Creek interchange through Moss Park Road, and again at the SR 528 and SR 417 Interchange.

Drainage structure obstructions and roadside erosion were observed at various locations throughout the roadway without any concentration in any specific area.



**Figure 10** Overgrown structure in pond east of SR 528.

### 2.3.2 SR 417 Central Florida GreeneWay Bridges

Bridge observations included overgrown vegetation in and along drainage flumes and slope pavement, bridge coatings that were peeling and fading on walls and beams and intermittent asphalt spalling and expansion joint tearing.

Expansion joint tearing and intermittent spalling was identified at SR 417 and SR 50.

Overgrown vegetation was observed along drainage flumes and slope pavement in various locations throughout the corridor.



**Figure 11** Peeling beam coating Lee Vista Boulevard.

### 2.3.3 SR 417 Central Florida GreeneWay Walls and Fencing

In general SR 417 walls were in good standing with no noticeable issues outside of a small amount of graffiti.

Fencing observations for both ROW and retaining wall were noted at various locations including vegetation overgrown fence lines, and damaged or downed ROW fencings.

### 2.3.4 SR 417 Central Florida Greeneway Facilities

The SR 417 consists of 4 Mainline Toll Plazas (John Young, Boggy Creek, Curry Ford, and University), and 25 ramp plazas.

The facilities along SR 417 were generally in good condition.

Observations for various features in the facilities along SR 417 were typically for wiring, switchboards and breakers, receptacles, duct work, ceiling tiles, vents, and flooring.

Individual summaries of facilities along SR 417 are indicated in the Annual Facilities Inspection Reports included in this document.



**Figure 12** Building façade damage SR 417 Lee Vista on ramp (isolated occurrence).

## 2.4 SR 429 DANIEL WEBSTER WESTERN BELTWAY / WEKIVA PARKWAY FINDINGS (RATINGS 2 OR 3).

SR 429 inspection limits were at Seidel Road, the south end of the CFX's jurisdiction north and east to the Wekiva Parkway jurisdictional limits. See Appendix 6 for detailed inspection sheets, maps, and photos.

### 2.4.1 SR 429 Daniel Webster Western Beltway / Wekiva Parkway Roadway

SR 429 roadway observations included roadside erosion, drainage structure obstructions, and paved shoulder edge ravel.



**Figure 13** Drainage structure erosion southbound SR 429 at Franklin Street

Roadside erosion was observed on slopes and swales in various locations on the roadway with the heaviest concentration from New Independence to Franklin Street.

Drainage structure obstructions were observed at various locations throughout the roadway without any concentration in any specific area.

### 2.4.2 SR 429 Daniel Webster Western Beltway / Wekiva Parkway Bridges

Bridge observations were minimal with a small amount of asphalt spall at the approach slab at Palm Drive, and ponding on shoulder at the barrier wall at Story Road.

### 2.4.3 SR 429 Daniel Webster Western Beltway / Wekiva Parkway Walls and Fencing

Notable wall and fence observations included spalling and minor cracking, vegetation overgrown fence lines, and damaged or downed ROW fencings.



**Figure 14** Overgrown ROW fence line SR 429 south of Forest Lake Mainline Plaza.

Fencing observations for both ROW and retaining wall were noted at various locations including vegetation overgrown fence lines, and damaged or downed ROW fencings throughout the corridor.



#### 2.4.4 SR 429 Daniel Webster Western Beltway / Wekiva Parkway Facilities

SR 429 consists of 2 Mainline Toll Plazas (Independence and Forest Lake), 3 Toll Gantries (Ponkan, Coronado, and Mt. Plymouth), and 12 ramp plazas.

The facilities along SR 429 were generally in good to excellent condition.

Observations for various features in the facilities along SR 429 were typically for wiring, switchboards and breakers, receptacles, duct work, ceiling tiles, vents, and flooring. As well as aesthetic cleaning.



**Figure 15** Mold growing on parapet of SR 429 New Independence Plaza.

Individual summaries of facilities along SR 429 are indicated in the Annual Facilities Inspection Reports included in this document.

### 2.5 SR 451 FINDINGS (RATINGS 2 OR 3).

See Appendix 4 for detailed inspection sheets, maps, and photos.

#### 2.5.1 SR 451 Roadway

SR 451 roadway observations consisted of overgrown drainage structures with moderate erosion.

Drainage structure observations were concentrated from SR 414 to Johns Road.



**Figure 16** Overgrown drainage structure south of Johns Road.

#### 2.5.2 SR 451 Bridges

Bridge observations were minimal with a small amount of asphalt spall at the approach slab and MSE coping cracking at CR 437A, and Johns Road.

#### 2.5.3 SR 451 Walls and Fencing

No observations were noted on SR 451 walls and fencing.

#### 2.5.4 SR 451 Facilities

There are no facilities located on SR 451.

### 2.6 SR 453 FINDINGS (RATINGS 2 OR 3).

SR 453 inspection limits were from the south end at the SR 429 interchange north to SR 46. See Appendix 7 for detailed inspection sheets, maps, and photos

#### 2.6.1 SR 453 Roadway

SR 453 observations were minor washouts and overgrown drainage structures.

#### 2.6.2 SR 453 Bridges

No observations were noted on SR 453 bridges.



### 2.6.3 SR 453 Walls and Fencing

Minor fence damage from fallen trees was observed in 3 places throughout the corridor.

### 2.6.4 SR 453 Facilities

No observations were noted on SR 453 facilities.

## 2.7 SR 528 MARTIN ANDERSEN BEACHLINE EXPRESSWAY FINDINGS (RATINGS 2 OR 3).

See Appendix 8 for detailed inspection sheets, maps, and photos.

### 2.7.1 SR 528 Martin Andersen Beachline Expressway Roadway

SR 528 roadway observations included roadside debris, bank erosion around drainage structures, drainage structure obstructions both vegetative and sediments, and general roadside erosion.

Roadside debris was observed adjacent to SR 528 east of Beachline Main Plaza.



**Figure 17** Roadside erosion adjacent to SR 528 east of Beachline Main.

Drainage structure obstructions and erosion were observed at various locations throughout the roadway with concentrated areas at the SR 436 interchange and Goldenrod Road Interchange.

### 2.7.2 SR 528 Martin Andersen Beachline Expressway Bridges

Bridge observations included spalling and delamination, vegetation growth in wall panels, sediment filled and overgrown wall gutters, missing reflectors, graffiti, and gaps at wing wall and slope pavement.

Minor spalling and delamination was identified on the beam and columns at Daetwyler Road.

Overgrown wall gutters were observed at Daetwyler Road.

Missing reflectors, graffiti, and gaps at wing wall and slope pavement were observed at the Farm Road access bridge.



**Figure 18** Overgrown wall gutter at SR 528 and Daetwyler Road.

### 2.7.3 SR 528 Martin B. Andersen Beachline Expressway Walls and Fencing

No observations were noted on SR 528 walls minor fence damage and overgrown fence lines was observed throughout the corridor.

### 2.7.4 SR 528 Martin B. Andersen Beachline Expressway Facilities

The SR 528 consists of 2 Mainline Toll Plazas (Beachline and Dallas), and 8 ramp plazas.

The facilities along SR 528 are generally in good condition.

Observations for various features in the facilities along SR 528 were typically for wiring,

switchboards and breakers, receptacles, duct work, ceiling tiles, vents, and flooring.

Individual summaries of facilities along SR 528 are indicated in the Annual Facilities Inspection Reports included in this document.



**Figure 19** Mold and mildew on canopy of Dallas Mainline Plaza pedestrian bridge.

## 2.8 NON-SYSTEM FACILITIES (RATINGS 2 OR 3).

See Appendix 8 for detailed inspection sheets, maps, and photos.

### 2.8.1 Goldenrod Road Extension

Goldenrod Road roadway observations included faded pavement markings and depressions in miscellaneous asphalt.

Faded pavement markings were observed at the Mainline Toll Plaza.

A large depression / hole was observed in the miscellaneous asphalt against the guardrail along slope pavement just northbound, just south of the Goldenrod Mainline Plaza.

### 2.8.2 Goldenrod Road Walls and Fencing

No observations were noted on Goldenrod Road walls and fencing.

### 2.8.3 Goldenrod Mainline Toll Plaza

The Goldenrod Road facility is generally in good condition.



**Figure 20** Depression along slope pavement at Goldenrod Mainline Plaza.

## 3.0 Projects Completed Since 2017 Inspection

Listed below are Renewal and Replacement Projects completed (July 1, 2017 to June 30, 2018) since the 2017 Annual Inspection.

### 3.1 ROADWAY

- Systemwide Line Dynamic Message Sign Upgrades
- Systemwide E-Pass Signage Improvements
- Goldenrod Milling and Resurfacing Lee Vista Boulevard to Narcoossee Road.
- Systemwide Logo Panel Replacement and Service Signage
- SR 408 Milling and Resurfacing Lake Underhill to Yucatan Drive.

### 3.2 FACILITIES

- Systemwide Generator Replacements Phase I
- SR 417 Toll Plaza Roof Replacements
- CFX Administration and Operations Center Security Systems Upgrades.



## 4.0 Summary

Overall, the CFX System has been well maintained and in good repair, working order, and condition. The overall condition of the System shows CFX's commitment to funding, maintaining, and operating a safe mobility network.

CFX reported an overall MRP score of 92. There are currently 335 bridge structures on the

CFX system with a rating of "Excellent or Good" for a Bridge Condition Rating of 98.7%, as well as, a Pavement Condition Rating of 92% of CFX maintained lane miles currently Good or higher.

Continued routine maintenance and implementation of Renewal and Replacement Projects will ensure CFX continues to provide the region with a world-class integrated mobility network.





## Appendix 1: Bridge Ratings



# Central Florida Expressway Authority Bridge Ratings

## Using FDOT Criteria

### Summary Key

#### Deck Rating, Superstructure Rating, Substructure Rating

**N** Not Applicable

**9** Excellent Condition

**8** Very Good Condition

**7** Good Condition

**6** Satisfactory Condition

**5** Fair Condition

**4** Poor Condition

**3** Serious Condition

**2** Critical Condition

**1** "Imminent" Failure

**0** Failed

#### Channel Rating

**N** Not Applicable

**9** Excellent Condition

**8** Very Good Condition

**7** Good Condition

**6** Satisfactory Condition

**5** Fair Condition

**4** Poor Condition

**3** Serious Condition

**2** Critical Condition

**1** "Imminent" Failure

**0** Failed

#### Culvert Rating

**N** Not Applicable

**9** No Deficiencies

**8** No Noticeable Deficiencies

**7** Minor Deficiencies

**6** Deterioration

**5** Moderate to Major Deterioration

**4** Large Spalls, Heavy Scaling, Wide Cracks

**3** Excessive Large Spalls, Heavy Scaling, Wide Cracks

**2** Corrective Action Required to Maintain Traffic

**1** Bridge Closed

**0** Replacement Necessary

#### Structure Rating

**1** Excellent

**2** Good

**3** Fair

**4** (and above) Poor

Central Florida Expressway Authority Bridge Ratings Summary  
Using FDOT Criteria

Bridge Number	Facility	Deck Rating	Superstructure Rating	Substructure Rating	Channel Rating	Culvert Rating	Structure Rating
110603	SR-429 (Wekiva Pkw	8	8	8	N	N	1
110604	SR-429 SB	8	8	8	N	N	1
750055	SR-528	N	N	N	6	7	2
750056	SR-528	6	7	6	N	N	2
750057	SR-528 WB	7	7	5	5	N	3
750058	SR-528	6	7	7	N	N	2
750059	SR-528 WB	7	7	7	N	N	2
750100	SR-408 WB	7	7	7	N	N	2
750102	SR-408 WB	7	7	7	N	N	2
750103	SR-408 WB	7	7	7	N	N	2
750104	SR-408 WB	7	7	7	N	N	2
750106	SR-408	7	7	7	N	N	2
750107	SR-408 WB	7	7	7	N	N	2
750108	SR-408 WB	7	7	7	N	N	2
750112	SR-408 Exit Ramp	7	7	7	N	N	2
750114	SR-408	6	7	7	7	N	2
750116	SR-408 WB	7	7	7	N	N	2
750119	SR-408 WB	7	7	7	N	N	2
750120	SR-408	7	7	7	N	N	2
750121	SR-408 WB	7	7	7	8	N	2
750123	SR-408 WB	7	7	7	N	N	2
750124	SR-408 WB	7	7	7	N	N	2
750126	SR-408 WB	7	7	7	N	N	2
750128	SR-408 WB	7	7	7	N	N	2
750129	SR-408	7	7	7	N	N	2
750130	I-4 Con. to SR-408	7	7	7	N	N	2
750137	I-4 Connector	7	7	7	N	N	2
750138	I-4 Connector	7	7	7	N	N	2
750179	SR-528	6	7	7	N	N	2
750182	SR-408 EB	7	7	7	N	N	2
750183	SR-408	6	7	7	9	N	2
750184	SR-408	7	7	7	N	N	2
750185	SR-408 EB	7	7	7	N	N	2
750186	SR-408 EB	7	7	7	N	N	2
750212	SR-528	7	7	5	5	N	3
750213	SR-528	7	7	7	N	N	2
750214	SR-528	7	7	7	N	N	2
750220	SR-408 EB	7	7	7	N	N	2
750231	SR-408 EB	7	7	7	N	N	2
750232	SR-408	7	7	7	N	N	2
750233	SR-408 EB	7	7	7	N	N	2
750234	SR-408 EB	7	7	7	N	N	2
750235	SR-408	7	7	7	N	N	2
750236	SR-408 EB	7	7	7	N	N	2



Central Florida Expressway Authority Bridge Ratings Summary  
Using FDOT Criteria

Bridge Number	Facility	Deck Rating	Superstructure Rating	Substructure Rating	Channel Rating	Culvert Rating	Structure Rating
750237	SR-408 EB	7	7	7	N	N	2
750238	SR-408	7	7	7	N	N	2
750239	SR-408	7	7	7	N	N	2
750240	SR-408	7	7	7	N	N	2
750241	SR-408	7	7	7	N	N	2
750242	SR-408	7	7	7	N	N	2
750243	SR-408	7	7	7	N	N	2
750244	SR-408	7	7	7	N	N	2
750245	SR-408	7	7	7	N	N	2
750246	SR-408 EB	7	7	7	N	N	2
750247	SR-408 EB	7	7	7	7	N	2
750248	SR-408 EB	7	7	7	N	N	2
750249	SR-408 EB	7	7	7	N	N	2
750251	SR-408 WB	7	7	7	N	N	2
750252	SR-408 EB	7	7	7	N	N	2
750253	SR-408	7	7	7	N	N	2
750300	SR-528	N	N	N	7	6	2
750315	SR-436	5	7	7	N	N	3
750316	SR-436	5	7	7	N	N	3
750317	SR-436 NB	6	7	7	N	N	2
750318	SR-528	9	7	7	N	N	2
750319	SR-528	8	7	7	N	N	1
750320	SR-528	8	7	7	N	N	2
750330	SR-528	N	N	N	7	6	2
750332	SR-528	7	7	7	N	N	2
750333	SR-528	7	7	7	N	N	2
750337	Chickasaw Trail	7	7	8	N	N	2
750342	SR-417 NB	7	7	7	N	N	2
750343	SR-417 SB	7	7	7	N	N	2
750344	SR-417 SB	7	7	7	7	N	2
750345	SR-417 NB	7	7	7	8	N	2
750346	Trevarthon Rd.	7	7	7	N	N	2
750347	Econlockhatcee Trl	7	8	8	N	N	2
750348	SR-417	7	7	7	N	N	2
750349	SR-417 NB	7	7	7	N	N	2
750350	SR-417 SB	7	7	7	N	N	2
750351	SR-417 NB	7	7	7	N	N	2
750352	SR-408 Ramp AR-2	7	8	7	N	N	2
750353	SR-408 Ramp AR-1	7	7	7	N	N	2
750354	SR-417 SB	7	7	7	N	N	2
750355	SR-417 NB	7	7	7	N	N	2
750356	Econlockhatcee Tr	7	7	7	N	N	2
750357	SR-408 WB	7	8	7	7	N	2
750358	SR-408 EB	7	7	7	7	N	2

Central Florida Expressway Authority Bridge Ratings Summary  
Using FDOT Criteria

Bridge Number	Facility	Deck Rating	Superstructure Rating	Substructure Rating	Channel Rating	Culvert Rating	Structure Rating
750359	Dean Road	7	7	7	N	N	2
750360	SR-408 WB	7	7	7	N	N	2
750361	SR-408 EB	7	7	7	N	N	2
750362	SR-408 WB	7	8	7	N	N	2
750363	SR-408 EB	7	8	7	N	N	2
750364	Woodbury Road	7	7	7	N	N	2
750365	SR-408 WB	7	7	8	N	N	2
750366	SR-408 EB	7	7	7	N	N	2
750369	SR-408 EB	8	8	7	N	N	2
750370	SR-408 WB	8	8	8	N	N	1
750373	SR-417 Ramp SB	7	7	7	N	N	2
750374	SR-417 NB Ramp	7	7	7	N	N	2
750375	SR-417 SB	7	7	7	N	N	2
750376	SR-417 NB	7	7	7	N	N	2
750377	SR-417	N	N	N	7	6	2
750378	SR-417 SB	7	7	7	N	N	2
750379	SR-417 NB	8	7	7	N	N	2
750380	SR-417 SB	7	8	7	N	N	2
750381	SR-417 NB	8	8	8	N	N	1
750382	SR-417 SB	7	7	7	N	N	2
750383	SR-417 NB	8	8	8	N	N	1
750384	SR-417 SB	7	7	8	N	N	2
750385	SR-417 NB	7	7	7	N	N	2
750390	SR-408 EB Ramp	8	7	8	N	N	2
750391	SR-408 WB Ramp	7	7	7	N	N	2
750392	Good Homes Road	7	7	7	N	N	2
750393	SR-408 WB	7	7	7	7	N	2
750394	SR-408 EB	7	7	7	7	N	2
750395	Dorscher Road	7	8	7	N	N	2
750396	SR-408 WB	7	7	7	N	N	2
750397	SR-408	7	7	7	N	N	2
750398	Powers Drive	7	6	8	N	N	2
750399	Paul Street	7	7	8	N	N	2
750406	SR-429 SB	7	8	7	N	N	2
750407	SR-429 NB	7	8	7	N	N	2
750408	SR-429 Ramp L	7	8	7	N	N	2
750409	SR-429 SB	7	8	7	N	N	2
750410	SR-429 NB	7	7	7	N	N	2
750411	Ramp A-NB Exit Ram	7	8	7	N	N	2
750412	SR-429 SB	8	8	8	N	N	1
750413	SR-429 NB	8	7	8	N	N	1
750414	SR-429 SB	8	7	7	N	N	2
750415	SR-429 NB	7	8	7	N	N	2
750416	SR-429 SB	8	8	8	7	N	1

Central Florida Expressway Authority Bridge Ratings Summary  
Using FDOT Criteria

Bridge Number	Facility	Deck Rating	Superstructure Rating	Substructure Rating	Channel Rating	Culvert Rating	Structure Rating
750417	SR-429 NB	8	8	8	7	N	1
750418	SR-429 SB	8	8	8	N	N	1
750419	SR-429 NB	8	8	8	N	N	1
750420	SR-429 SB	7	8	8	N	N	2
750421	SR-429 NB	8	8	8	N	N	1
750422	West Road	8	7	8	N	N	2
750423	West Road	8	8	8	N	N	1
750424	SR-417 NB	7	7	8	N	N	2
750425	SR-417 SB	7	7	8	N	N	2
750426	SR-417 SB	7	7	7	7	N	2
750427	SR-417 NB	7	7	7	7	N	2
750428	SR-417 SB	7	7	8	N	N	2
750429	SR-417 NB	7	7	8	N	N	2
750430	SR-417 SB	7	8	7	N	N	2
750431	SR-417 NB	7	7	7	N	N	2
750432	SR-417 Off Ramp	7	7	7	N	N	2
750433	SR-417 SB	7	7	8	N	N	2
750434	SR-417 NB	7	8	8	N	N	2
750435	SR-417 SB Off Ramp	7	8	8	N	N	2
750436	SR-417 SB	7	7	7	N	N	2
750437	SR-417 NB	7	7	7	N	N	2
750438	SR-417 SB	7	8	7	N	N	2
750439	SR-417 NB	7	8	7	N	N	2
750440	SR-417 SB	7	7	6	N	N	2
750441	SR-417 NB	7	7	7	N	N	2
750442	SR-417 SB	7	7	7	N	N	2
750443	SR-417 NB	7	7	7	N	N	2
750444	SR-417 NB Off Ramp	8	7	7	N	N	2
750445	SR-417 SB	7	7	7	N	N	2
750446	SR-417 NB	7	7	7	N	N	2
750447	SR-417 SB	7	7	7	N	N	2
750448	SR-417 NB	7	7	8	N	N	2
750449	SR-417 SB Ramp	7	8	7	7	N	2
750450	SR-417	7	7	7	7	N	2
750451	SR-417	7	7	7	7	N	2
750452	SR-417 Off Ramp	7	8	7	7	N	2
750453	SR-417	7	7	7	N	N	2
750454	SR-417	7	7	7	N	N	2
750457	SR-417	8	8	7	N	N	2
750458	SR-417	8	8	7	N	N	2
750459	SR-417 SB	6	7	7	N	N	2
750460	SR-417	6	7	7	N	N	2
750461	SR-417 NB	8	7	7	N	N	2
750462	SR-417 SB	7	7	7	N	N	2



Central Florida Expressway Authority Bridge Ratings Summary  
Using FDOT Criteria

Bridge Number	Facility	Deck Rating	Superstructure Rating	Substructure Rating	Channel Rating	Culvert Rating	Structure Rating
750463	SR-417 SB	7	8	7	N	N	2
750464	SR-417 NB	8	7	7	N	N	2
750465	SR-417	7	7	7	N	N	2
750466	SR-417	7	7	7	N	N	2
750467	SR-417	7	7	8	N	N	2
750468	SR-417	7	7	7	N	N	2
750469	SR-417 NB Off Ramp	7	8	8	N	N	2
750470	SR-417 Ramp C	7	7	7	N	N	2
750471	Ramp To SR-528 WB	7	7	7	N	N	2
750472	SR-417	7	7	8	N	N	2
750473	SR-417	7	7	8	N	N	2
750480	SR-417	N	N	N	6	7	2
750481	SR-417	N	N	N	7	7	2
750492	SR-429 SB	8	8	8	N	N	1
750493	SR-429 NB	8	8	8	N	N	1
750494	SR-429 SB	8	8	7	N	N	2
750495	SR-429 NB	8	8	7	N	N	2
750496	SR-429 SB	7	8	7	N	N	2
750497	SR-429 NB	8	8	7	N	N	2
750502	SR-451 SB	8	8	8	N	N	1
750503	SR-451 NB	8	8	8	N	N	1
750504	Johns Road	8	8	8	N	N	1
750505	SR-451 SB	8	8	8	N	N	1
750506	SR-451 NB	8	8	8	N	N	1
750507	SR-451 SB	8	8	8	N	N	1
750508	SR-451	8	8	8	N	N	1
750509	Ramp C	8	8	7	N	N	2
750512	Goldenrod Rd.	7	7	7	N	N	2
750520	SR-429 SB	8	8	8	N	N	1
750521	SR-429 NB	8	8	8	N	N	1
750522	SR-429 SB	8	8	8	N	N	1
750523	SR-429 NB	8	8	8	N	N	1
750524	SR-429 SB	8	8	8	N	N	1
750525	SR-429 NB	8	8	8	N	N	1
750526	SR-429 SB	8	8	8	N	N	1
750527	SR-429 NB	8	8	8	N	N	1
750528	SR-429 SB	8	8	8	N	N	1
750529	SR-429 NB	8	8	8	N	N	1
750530	Malcolm Rd.	8	7	8	N	N	2
750531	SR-429 SB	8	8	8	N	N	1
750532	SR-429 NB	8	8	8	N	N	1
750533	SR-429 SB	8	8	7	N	N	2
750534	SR-429 NB	8	8	7	N	N	2
750535	SR-429 SB	8	8	8	N	N	1

Central Florida Expressway Authority Bridge Ratings Summary  
Using FDOT Criteria

Bridge Number	Facility	Deck Rating	Superstructure Rating	Substructure Rating	Channel Rating	Culvert Rating	Structure Rating
750536	SR-429 NB	8	8	7	N	N	2
750537	SR-429 SB	8	8	8	N	N	1
750538	SR-429 NB	8	8	8	N	N	1
750539	SR-429 SB	8	7	8	N	N	2
750540	SR-429 NB	8	7	8	N	N	2
750541	SR-429 SB	8	8	8	N	N	1
750542	SR-429 NB	8	8	8	N	N	1
750543	SR-429 SB	8	7	7	N	N	2
750544	SR-429 NB	8	8	8	N	N	1
750547	SR-429 SB	7	7	8	N	N	1
750548	SR-429 NB	8	8	8	N	N	1
750549	SR-429 SB	7	8	8	N	N	1
750550	SR-429 NB	7	8	8	N	N	1
750553	SR-429 SB	7	7	8	8	N	2
750554	SR-429 NB	8	8	7	6	N	2
750557	NB C-D Road Ramp	8	7	7	N	N	2
750567	New Indepence Pkwy	8	7	8	N	N	2
750569	SR-408 EB Ramp C	7	7	8	N	N	2
750570	SR-408WB to I-4EB	7	8	8	N	N	2
750571	SR-408WB to I-4EB	7	8	8	N	N	2
750576	SR-417 NB Ramp B1	8	8	8	N	N	1
750579	Lake Underhill Dr.	8	8	8	N	N	1
750580	SR-408 EB	8	8	8	N	N	1
750581	SR-408	8	8	8	N	N	1
750589	Conway Rd Ramp	8	8	8	N	N	1
750701	SR-408 WB	7	8	8	N	N	2
750703	SR-414	8	8	8	N	N	1
750704	SR-414	8	8	8	N	N	1
750705	SR-414	8	8	8	N	N	1
750706	SR-414	8	8	8	N	N	1
750707	SR-414	7	7	7	8	N	2
750708	SR-414	8	8	8	N	N	1
750709	SR-414	8	8	8	N	N	1
750710	SR-44 WB Ramp	8	8	8	N	N	1
750711	SR-414 EB Ramp	8	8	8	N	N	1
750712	SR-414	8	8	8	N	N	1
750714	SR-429 SB	8	8	8	N	N	1
750715	SR-429 NB	8	8	8	N	N	1
750716	SR-429 SB	8	8	8	N	N	1
750717	SR-429 NB	8	8	8	N	N	1
750718	SR-414 Ramp CA	8	8	8	N	N	1
750719	SR-429 NB Ramp BF	8	8	8	N	N	1
750720	SR-414 WB	8	7	8	N	N	2
750721	SR-414 EB	8	8	8	N	N	1

Central Florida Expressway Authority Bridge Ratings Summary  
Using FDOT Criteria

Bridge Number	Facility	Deck Rating	Superstructure Rating	Substructure Rating	Channel Rating	Culvert Rating	Structure Rating
750722	SR-429 NB Ramp EG	8	8	8	N	N	1
750723	SR-451 NB Ramp	7	7	7	N	N	2
750724	Maitland Blvd Ext.	8	8	8	N	N	1
750725	Marden Rd	7	7	7	N	N	2
750726	SR-414	7	7	7	N	N	2
750727	SR-414	8	8	8	N	N	1
750728	SR-414 Ramp CA	8	7	8	N	N	2
750729	Ramp K	8	8	8	N	N	1
750730	Ramp K	7	8	8	N	N	1
750731	CR-437 Binion Road	8	8	8	N	N	1
750732	SR-429 SB	8	8	8	N	N	1
750733	SR-429 NB	8	8	8	N	N	1
750734	SR-429 SB	8	8	8	N	N	1
750735	SR-429 NB	8	8	8	N	N	1
750736	SR-429 Viaduct SB	8	8	8	N	N	1
750737	SR-429 NB	8	8	8	N	N	1
750738	SR-429 SB	9	9	9	N	N	1
750739	SR-429 NB	9	9	9	N	N	1
750741	SR-408 WB On-Ramp	8	8	8	N	N	1
750742	SR-408 EB Off-Ramp	8	8	8	N	N	1
750743	SR-414	8	8	8	N	N	1
750802	Airport to SR417	8	7	8	9	N	1
750803	SR-417 NB Ramp J	8	8	8	N	N	1
750804	Airport to SR417SB	8	7	8	9	N	2
750805	South Access Rd.	8	8	8	N	N	1
750806	SR417 NB to Airpor	8	7	7	9	N	2
750807	SR 528 WB	7	8	8	N	N	2
750808	SR-528 EB	8	8	8	N	N	1
750823	Ramp G	8	8	8	8	N	1
750824	SR-417 Ramp G	8	7	8	N	N	2
750825	Ramp D Flyover	8	8	8	N	N	1
750829	SR-528 Ramp BC-1	8	8	8	N	N	1
750830	SR-528 Ramp BC-2	8	8	8	N	N	1
750839	Ramp D2	9	9	9	N	N	1
750840	Ramp C2	9	9	9	N	N	1
750841	Ramp C1	8	8	8	N	N	1
750850	Ramp B2 (SR-417)	8	8	8	N	N	1



## Appendix 2: Facility Reports





## 2018 Annual Facilities Inspection Report



### Hiawassee Main Plaza

October 15, 2018

SUBMITTED BY:

**Dewberry Engineers Inc.**

800 N. Magnolia Avenue, Suite 1000  
Orlando, Florida 32803  
407.843.5120

SUBMITTED TO:

**Central Florida Expressway Authority**

Corporate Office  
4974 ORL Tower Road  
Orlando, Florida 32807  
407.690.5000

**Attn: Robert Glasemann  
Facilities Supervisor  
Central Florida Expressway Authority  
4974 ORL Tower Road  
Orlando, FL 32807**

Inspection Date: 10/25/18  
Arrived: 7:30 AM

Dewberry Engineers Inc. (Dewberry) as part of the Annual System Inspection performed the annual building inspection of the Hiwassee Mainline Plaza and associated ramps.

### **43 – Hiwassee Main Plaza**

#### **Summary of Noted Observations:**

The above referenced site is in good condition overall with no identified deficiencies. Maintenance is recommended on some isolated items as referenced below. Please refer to inspection photos for visual observations. The following items were observed:

#### **Interior Observations:**

- Mainline computer room needs maintenance to repair broken door latch. (Figure 1)
- Mainline computer room repairs to attach unistrut flexible conduit and electrical box properly to the wall. (Figure 2)
- Mainline computer room repairs to attach fire alarm electrical box properly to the wall. (Figure 3)
- Mainline computer room cover is needed for electrical trough. (Figure 4)
- Mainline computer room needs to replace missing HVAC vent. (Figure 5)
- Mainline breakroom sink needs repairs to fix counter separation to wall. (Figure 6)

#### **Tunnel Observations:**

- The above referenced tunnel is in good condition overall with no identified deficiencies.

#### **Exterior Observations:**

- Equipment covers located in area behind building exterior need to be fastened securely. (Figure 7 & Figure 8)
- Refrigerant line connection to HVAC unit behind building needs repairs to correct leaking connection. (Figure 9 & Figure 10)

#### **Recommendations:**

- Recommend HVAC contractor to repair leaking connection on HVAC unit and replace missing vent in computer room.
- Recommend maintenance on door latch in computer room.
- Recommend repairs to attach fire alarm electrical box in computer room.
- Recommend electrical contractor fix the electrical box, conduit & unistrut in the computer room.



**41 – Good Homes Ramp – On Observations:**

The above referenced on ramp is in good condition overall. Maintenance on the UPS is recommended. (Figure 11 & Figure 12)

**42 – Good Homes Ramp – Off Observations:**

The above referenced on ramp is in good condition overall. Maintenance to repair what appears to be water intrusion is recommended. (Figure 13 & Figure 14)

**44 – Hiwassee Ramp – On Observations:**

The above referenced on ramp is in good condition overall. (Figure 15 & Figure 16)

**45 – Hiwassee Ramp – Off Observations:**

The above referenced on ramp is in good condition overall. Maintenance on the keypad to the computer room is recommended. (Figure 17 & Figure 18)

All notes and items in this report are a record of observations provided by the site visit. Please find attached to this report, photos and items observed.

Prepared By: Julie A Andrews



Figure 1 Hiawassee Mainline



Figure 2 Hiawassee Mainline



Figure 3 Hiawassee Mainline



Figure 4 Hiawassee Mainline



Figure 5 Hiawassee Mainline

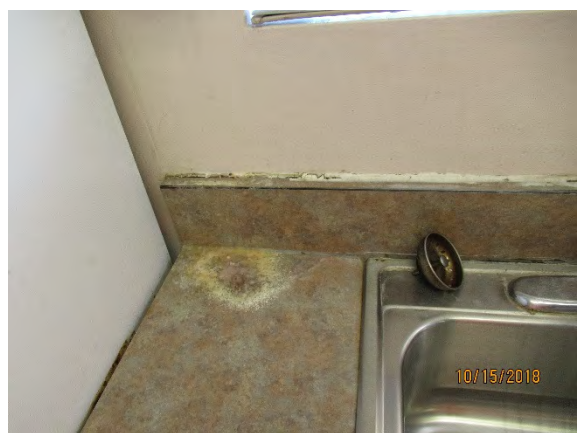


Figure 6 Hiawassee Mainline





Figure 7 Hiwassee Mainline Exterior



Figure 8 Hiwassee Mainline Exterior



Figure 9 Hiwassee Mainline Exterior



Figure 10 Hiwassee Mainline Exterior



Figure 11 Good Homes Ramp On



Figure 12 Good Homes Ramp On





Figure 13 Good Homes Ramp Off



Figure 14 Good Homes Ramp Off



Figure 15 Hiawassee Ramp Off



Figure 16 Hiawassee Ramp Off



Figure 17 Hiawassee Ramp On

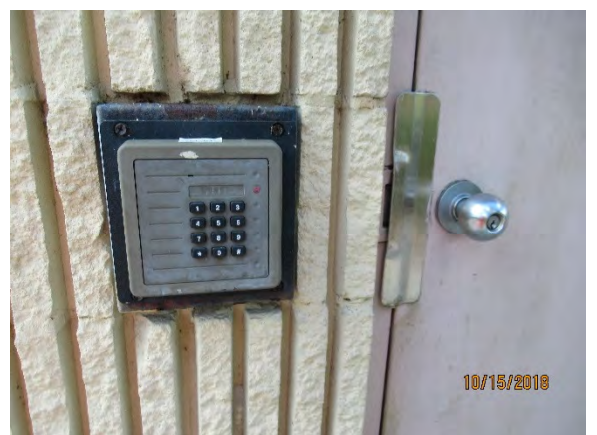


Figure 18 Hiawassee Ramp On

## 2018 Annual Facilities Inspection Report



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### Pine Hills Main Plaza

October 25, 2018

SUBMITTED BY:

**Dewberry Engineers Inc.**

800 N. Magnolia Avenue, Suite 1000  
Orlando, Florida 32803  
407.843.5120

SUBMITTED TO:

**Central Florida Expressway Authority**

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4974 ORL Tower Road  
Orlando, Florida 32807  
407.690.5000

**Attn: Robert Glasemann  
Facilities Supervisor  
Central Florida Expressway Authority  
4974 ORL Tower Road  
Orlando, FL 32807**

Inspection Date: 10/25/18  
Arrived: 7:30 AM

Dewberry Engineers Inc. (Dewberry) as part of the Annual System Inspection performed the annual building inspection of the Pine Hills Mainline Plaza and associated ramps.

#### **46 – Pine Hills Main Plaza**

##### **Summary of Noted Observations:**

The above referenced site is in good condition overall with no identified deficiencies. Maintenance is recommended on some isolated items as referenced below. Please refer to inspection photos for visual observations. The following items were observed:

##### **Interior Observations:**

- Mainline computer room ceiling tile has potential leak above ceiling. (Figure 1)
- Mainline computer room enclosure equipment not securely fastened to backboard. (Figure 2)
- Mainline computer room has isolated wiring bundles with multiple power strips on floor below equipment racks. (Figure 5)
- Mainline computer room is missing HVAC vent covers. (Figure 6)

##### **Overpass Observations:**

- The above referenced overpass has rusted members identified. (Figure 7 & Figure 8)

##### **Exterior Observations:**

- Elevator machine room ventilation is in need of repair. (Figure 3 & Figure 4)

##### **Recommendations:**

- Recommend isolated wiring in computer room be updated with rack mounted power strips.
- Recommend maintenance determine the source of any leaks above the ceiling grid.
- Recommend HVAC contractor to ensure that all vents have covers installed.
- Recommend repairs to ventilation system in elevator machine room.
- Recommend routine maintenance on overpass.



**47 – Old Winter Garden Ramp – Off Observations:**

The above referenced on ramp is in good condition overall. (Figure 9 & Figure 10)

**48 – Ortman/Mercy Drive Ramp – On Observations:**

The above referenced on ramp is in good condition overall. Maintenance is recommended on some ceiling tiles and potential leaks need to be addressed. (Figure 11 & Figure 12)

**49 – John Young Parkway Ramp – Off Observations:**

The above referenced on ramp is in good condition overall. Maintenance to repair tears in ductwork is recommended. (Figure 15 & Figure 16)

**50 – John Young Parkway Ramp – On Observations:**

The above referenced on ramp is in good condition overall. General maintenance is recommended on the generator annunciator panel. (Figure 13 & Figure 14)

**51 – OBT Ramp – Off Observations:**

The above referenced on ramp is in good condition overall. Some landscaping around the bollards and propane tank is recommended (Figure 13 & Figure 14)

**52 – OBT Ramp – On Observations:**

The above referenced on ramp is in good condition overall. Maintenance to repair building façade showing separation from building and broken door latch is recommended.  
(Figure 19, Figure 20, Figure 21 & Figure 22)

**53 – Mills Ramp – Off Observations:**

The above referenced on ramp is in good condition overall. Maintenance is recommended to properly fireproof ceiling penetrations and cap off communication wiring at grade.  
(Figure 23, Figure 24, Figure 25 & Figure 26)

All notes and items in this report are a record of observations provided by the site visit. Please find attached to this report, photos and items observed.

Prepared By: Julie A Andrews



Figure 1 Pine Hills Mainline IT Room



Figure 2 Pine Hills Mainline IT Room



Figure 3 Pine Hills Mainline



Figure 4 Pine Hills Mainline



Figure 5 Pine Hills Mainline



Figure 6 Pine Hills Mainline





Figure 7 Pine Hills Mainline



Figure 8 Pine Hills Mainline



Figure 9 Old Winter Garden Ramp Off



Figure 10 Old Winter Garden Ramp Off



Figure 11 Ortman Mercy Drive Ramp On



Figure 12 Ortman Mercy Drive Ramp On





Figure 13 John Young Parkway Ramp On



Figure 14 John Young Parkway Ramp On



Figure 15 John Young Parkway Ramp Off



Figure 16 John Young Parkway Ramp Off



Figure 17 OBT Ramp Off



Figure 18 OBT Ramp Off





Figure 19 OBT Ramp On



Figure 20 OBT Ramp On



Figure 21 OBT Ramp On



Figure 22 OBT Ramp On



Figure 23 Mills Ramp Off



Figure 24 Mills Ramp Off



*Figure 25 Mills Ramp Off*



*Figure 26 Mills Ramp Off*





## 2018 Annual Facilities Inspection Report



### Conway Main Plaza

November 08, 2018

SUBMITTED BY:

**Dewberry Engineers Inc.**

800 N. Magnolia Avenue, Suite 1000  
Orlando, Florida 32803  
407.843.5120

SUBMITTED TO:

**Central Florida Expressway Authority**

Corporate Office  
4974 ORL Tower Road  
Orlando, Florida 32807  
407.690.5000

**Attn: Robert Glasemann  
Facilities Supervisor  
Central Florida Expressway Authority  
4974 ORL Tower Road  
Orlando, FL 32807**

Inspection Date: 11/08/18  
Arrived: 8:00 AM

Dewberry Engineers Inc. (Dewberry) as part of the Annual System Inspection performed the annual building inspection of the Conway Mainline Plazas and associated ramps.

**58EB – Conway EB Main Plaza  
58EB – Conway WB Main Plaza**

**Summary of Noted Observations:**

The above referenced site is in good condition overall with no identified deficiencies. Maintenance is recommended on some isolated items as referenced below. Please refer to inspection photos for visual observations. The following items were observed:

**Interior Observations:**

- EB Mainline computer room electrical wiring, power strips and equipment on floor under equipment rack. (Figure 1)
- EB Mainline entry appears to have a leak at the ceiling tile/vent location. (Figure 2)
- WB Mainline computer room electrical wiring, power strips and equipment on floor under equipment rack. (Figure 3)
- WB Mainline missing vent cover in computer room. (Figure 4)

**Tunnel Observations:**

- EB Tunnel is in good condition overall with no identified deficiencies.
- WB Tunnel is in good condition overall with no identified deficiencies.

**Exterior Observations:**

- EB Plaza - Exterior Building has hairline cracks in one of the columns on the EB Plaza. (Figure 3)

**Recommendations:**

- Recommend maintenance on of isolated wiring for computer room.
- Recommend HVAC contractor to ensure all vents are clear and have vent covers.
- Recommend maintenance on building façade to address separation from building.

**54 – Bumby Ramp – On Observations:**

The above referenced on ramp is in good condition overall. (Figure 6 & Figure 7)

**55 – Bumby Ramp – Off Observations:**

The above referenced on ramp is in good condition overall. Some maintenance to repair exterior pedestal wiring is recommended. (Figure 7 & Figure 8)

**56 – Conway Ramp – On Observations:**

The above referenced on ramp is in good condition overall. (Figure 9 & Figure 10)

**57 – Conway Ramp – Off Observations:**

The above referenced on ramp is in good condition overall. Some maintenance is recommended to install trough cover and fasten 66 block securely to communications panel. (Figure 11, Figure 12, Figure 13 & Figure 14)

**59 – Andes Ave/Lake Underhill/436 Ramp – Off Observations:**

The above referenced on ramp is in good condition overall. Some general maintenance of the exterior debris is recommended (Figure 15 & Figure 16)

**60 – Semoran Ramp – Off Observations:**

The above referenced on ramp is in good condition overall. Some maintenance to repair exterior pedestal wiring, fireproof ceiling penetration exposed to outside and repair broken door is recommended. (Figure 19, Figure 20, Figure 21 & Figure 22)

**61 – Yucatan Ramp – Off Observations:**

The above referenced on ramp is in good condition overall. Some maintenance to securely fasten underground propane tank cover is recommended. (Figure 23 & Figure 24)

All notes and items in this report are a record of observations provided by the site visit. Please find attached to this report, photos and items observed.

Prepared By: Julie A Andrews



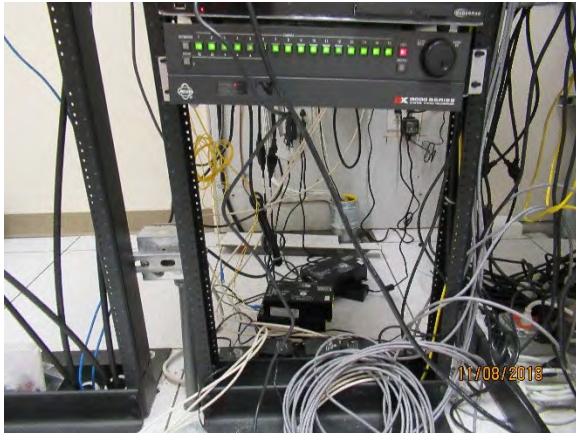


Figure 1 Conway EB Mainline IT Room



Figure 2 Conway EB Mainline Main Hall



Figure 3 Conway WB Mainline



Figure 4 Conway WB Mainline



Figure 5 Bumby Ramp On



Figure 6 Bumby Ramp On





Figure 7 Bumby Ramp Off



Figure 8 Bumby Ramp Off



Figure 9 Conway Ramp On

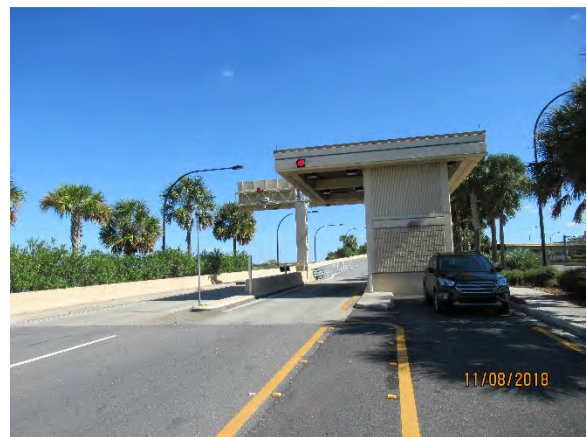


Figure 10 Conway Ramp On



Figure 11 Conway Ramp Off



Figure 12 Conway Ramp Off





Figure 13 Conway Ramp Off



Figure 14 Conway Ramp Off



Figure 15 Andes Ave/LkUndehill/436 Ramp Off



Figure 16 Andes Ave/LkUndehill/436 Ramp Off



Figure 17 Semoran Ramp Off

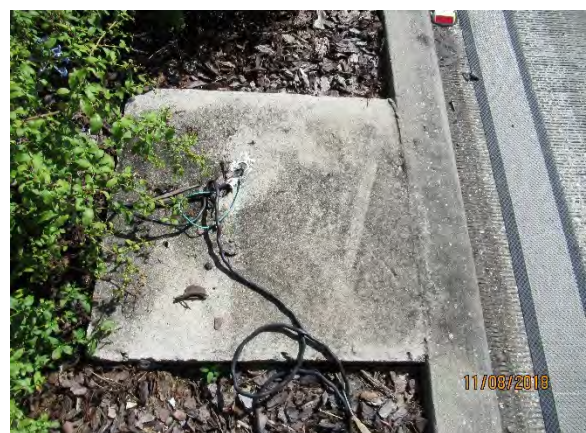


Figure 18 Semoran Ramp Off





Figure 19 Semoran Ramp Off



Figure 20 Semoran Ramp Off



Figure 21 Semoran Ramp Off



Figure 22 Semoran Ramp Off



Figure 23 Yucatan Ramp On



Figure 24 Yucatan Ramp On



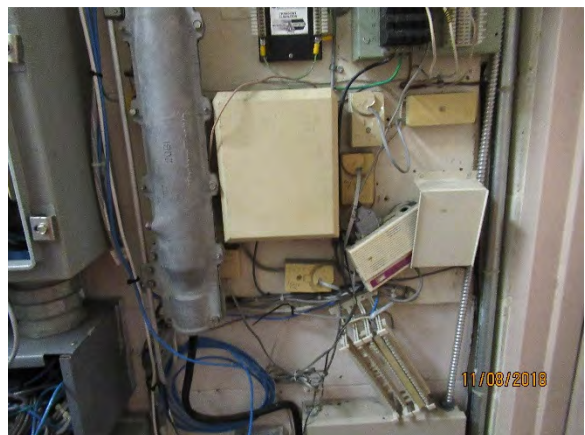
*Figure 19 Yucatan Ramp On*



*Figure 20 Yucatan Ramp On*



*Figure 21 Yucatan Ramp On*



*Figure 22 Yucatan Ramp On*





## 2018 Annual Facilities Inspection Report



### Dean Main Plaza

November 1, 2018

SUBMITTED BY:

**Dewberry Engineers Inc.**

800 N. Magnolia Avenue, Suite 1000  
Orlando, Florida 32803  
407.843.5120

SUBMITTED TO:

**Central Florida Expressway Authority**

Corporate Office  
4974 ORL Tower Road  
Orlando, Florida 32807  
407.690.5000



**Attn: Robert Glasemann  
Facilities Supervisor  
Central Florida Expressway Authority  
4974 ORL Tower Road  
Orlando, FL 32807**

Inspection Date: 11/01/18  
Arrived: 7:30 AM

Dewberry Engineers Inc. (Dewberry) as part of the Annual System Inspection performed the annual building inspection of the Dean Mainline Plaza and associated ramps.

## **64 – Dean Main Plaza**

### **Summary of Noted Observations:**

The above referenced site is in good condition overall with no identified deficiencies. Maintenance is recommended on some isolated items as referenced below. Please refer to inspection photos for visual observations. The following items were observed:

### **Interior Observations:**

- Mainline computer room equipment rack power cables. (Figure 1)

### **Tunnel Observations:**

- Mainline tunnel CMS control panel Lane 1 & Lane 2 NEMA enclosures rusted out. (Figure 2, Figure 3 & Figure 4)
- Mainline tunnel seams are showing deterioration. (Figure 5 & Figure 6)
- Mainline tunnel overhead wire pathway is not properly secured. (Figure 7 & Figure 8)
- Mainline tunnel control panel NEMA enclosures beginning to rust. (Figure 9 & Figure 10)

### **Exterior Observations:**

- Exterior Building has active construction site located behind building exterior that does not appear to be properly secured. (Figure 11 & Figure 12)

### **Recommendations:**

- Recommend electrical contractor replace the rusted enclosure on the tunnel control panels.
- Recommend general maintenance to locate/address leaks in ceiling tile.
- Recommend general maintenance to fix ceiling seam in tunnel.

**62 – Dean Road Ramp – On Observations:**

The above referenced on ramp is in good condition overall. Some maintenance is recommended on the damage to the building overhead E-pass lane and general electrical maintenance is recommended. (Figure 13, Figure 14, Figure 15 & Figure 16)

**63 – Dean Road Ramp – Off Observations:**

The above referenced on ramp is in good condition overall. Some maintenance to fireproof ceiling penetration and repairs to the separated guardrail is recommended.  
(Figure 17, Figure 18, Figure 19 & Figure 20)

**65 – Rouse Road Ramp – Off Observations:**

The above referenced on ramp is in good condition overall. Maintenance is recommended on the HVAC duct work that is attached to the ceiling and air conditioning units with duct tape.  
(Figure 21, Figure 22, Figure 23 & Figure 24)

**66 – Rouse Road Ramp – On Observations:**

The above referenced on ramp is in good condition overall. Some maintenance to fireproof ceiling penetration, remove interior debris and landscape areas in need is recommended.  
(Figure 25, Figure 26, Figure 27 & Figure 28)

All notes and items in this report are a record of observations provided by the site visit. Please find attached to this report, photos and items observed.

Prepared By: Julie A Andrews



Figure 1 Dean Mainline



Figure 2 Dean Mainline



Figure 3 Dean Mainline



Figure 4 Dean Mainline



Figure 5 Dean Mainline Dumb Waiter



Figure 6 Dean Mainline Tunnel





Figure 7 Dean Mainline Tunnel



Figure 8 Dean Mainline Tunnel

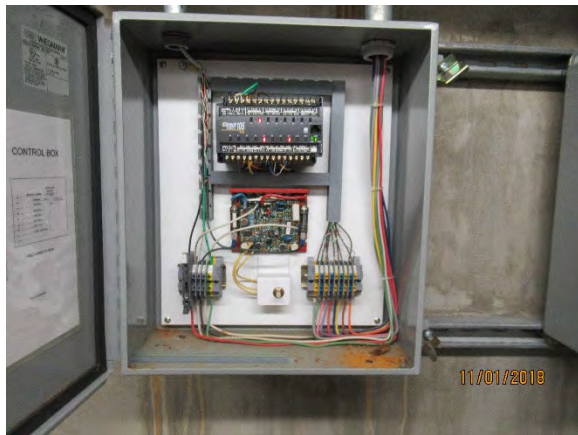


Figure 9 Dean Mainline Tunnel



Figure 10 Dean Mainline Tunnel



Figure 11 Dean Mainline Tunnel



Figure 12 Dean Mainline Exterior





Figure 13 Dean Road Ramp On



Figure 14 Dean Road Ramp On



Figure 15 Dean Road Ramp On



Figure 16 Dean Road Ramp On



Figure 17 Dean Road Ramp Off



Figure 18 Dean Road Ramp Off





Figure 19 Dean Road Off



Figure 20 Dean Road Off



Figure 21 Rouse Road Off



Figure 22 Rouse Road Off



Figure 23 Rouse Road Off



Figure 24 Rouse Road Off





Figure 25 Rouse Road On



Figure 26 Rouse Road On



Figure 27 Rouse Road On



Figure 28 Rouse Road On



Figure 29 Rouse Road On



Figure 30 Rouse Road On

## 2018 Annual Facilities Inspection Report



### Coral Hills Main Plaza

September 13, 2018

SUBMITTED BY:

**Dewberry Engineers Inc.**

800 N. Magnolia Avenue, Suite 1000  
Orlando, Florida 32803  
407.843.5120

SUBMITTED TO:

**Central Florida Expressway Authority**

Corporate Office  
4974 ORL Tower Road  
Orlando, Florida 32807  
407.690.5000

**Attn: Robert Glasemann**  
**Facilities Supervisor**  
**Central Florida Expressway Authority**  
**4974 ORL Tower Road**  
**Orlando, FL 32807**

Inspection Date: 09/13/18  
Arrived: 7:30 AM

Dewberry Engineers Inc. (Dewberry) as part of the Annual System Inspection performed the annual building inspection of the Coral Hills Mainline Plaza and associated ramps.

## **67 – Coral Hills Main Plaza**

### **Summary of Noted Observations:**

The above referenced site is in good condition overall with no identified deficiencies. Maintenance is recommended on some isolated items as referenced below. Please refer to inspection photos for visual observations. The following items were observed:

#### **Interior Observations:**

- Mainline has cracks in the glass windows in the manager's office and break room. (Figure 1, Figure 3 & Figure 4)
- Mainline computer room missing vent cover. (Figure 2)
- Mainline has potential leak above ceiling tile in computer room, men's restroom and recording room. (Figure 5, Figure 6 & Figure 7)

#### **Overpass Observations:**

- The above referenced overpass is in good condition overall with no identified deficiencies.

#### **Exterior Observations:**

- Exterior building has wasp nests along facade. (Figure 8)

#### **Recommendations:**

- Recommend examination of roof to locate any potential leaks
- Recommend pressure wash on exterior walkway.
- Recommend electrical contractor check the wiring issues and repair as required.



**68 – Keene Road Ramp – Off Observations:**

The above referenced on ramp is in good condition overall. (Figure 9 & Figure 10)

**69 – Keene Road Ramp – On Observations:**

The above referenced on ramp is in good condition overall. Some maintenance on the electrical wiring for the circuits without power is recommended. (Figure 11 & Figure 12)

**70 – Hiawassee Ramp – Off Observations:**

The above referenced on ramp is in good condition overall. Maintenance on ventilation system is recommended. (Figure 13 & Figure 14)

**71 – Hiawassee Ramp – On Observations:**

The above referenced on ramp is in good condition overall. Maintenance on ventilation system is recommended. (Figure 15, Figure 16, Figure 17 & Figure 18)

All notes and items in this report are a record of observations provided by the site visit. Please find attached to this report, photos and items observed.

Prepared By: Julie A Andrews



Figure 1 Coral Hills Mainline



Figure 2 Coral Hills Mainline

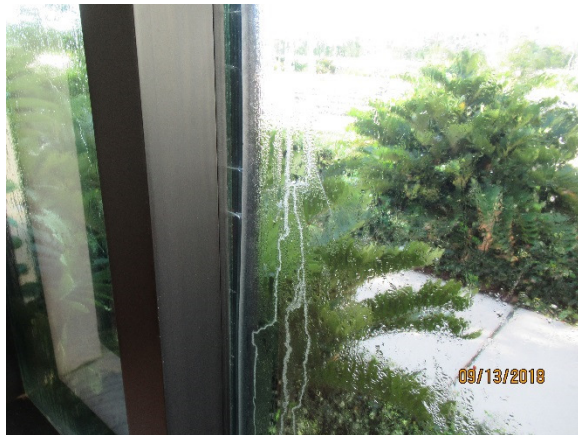


Figure 3 Coral Hills Mainline



Figure 4 Coral Hills Mainline



Figure 5 Coral Hills Mainline IT Room



Figure 6 Coral Hills Mainline Men's Room





Figure 7 Coral Hills Mainline Recording Room



Figure 8 Coral Hills Mainline



Figure 9 Keene Road Ramp Off



Figure 10 Keene Road Ramp Off



Figure 11 Keene Road Ramp On



Figure 12 Keene Road Ramp On





Figure 13 Hiawassee Ramp Off



Figure 14 Hiawassee Ramp Off



Figure 15 Hiawassee Ramp On



Figure 16 Hiawassee Ramp On



Figure 17 Hiawassee Ramp Off



Figure 18 Hiawassee Ramp Off



## 2018 Annual Facilities Inspection Report



John Young Main Plaza

July 12, 2018

SUBMITTED BY:

**Dewberry Engineers Inc.**

800 N. Magnolia Avenue, Suite 1000  
Orlando, Florida 32803  
407.843.5120

SUBMITTED TO:

**Central Florida Expressway Authority**

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4974 ORL Tower Road  
Orlando, Florida 32807  
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**Attn: Robert Glasemann  
Facilities Supervisor  
Central Florida Expressway Authority  
4974 ORL Tower Road  
Orlando, FL 32807**

Inspection Date: 07/12/18  
Arrived: 7:30 AM

Dewberry Engineers Inc. (Dewberry) as part of the Annual System Inspection performed the annual building inspection of the John Young Parkway Mainline Plaza and associated ramps.

## **1 – John Young Parkway Main Plaza**

### **Summary of Noted Observations:**

The above referenced site is in good condition overall with no identified deficiencies. Maintenance is recommended on some isolated items as referenced below. Please refer to inspection photos for visual observations. The following items were observed:

#### **Interior Observations:**

- Overhead vent missing vent cover in IT room. (Figure 1)

#### **Tunnel Observations:**

- Tunnel overhead conduit missing end cap. (Figure 2)

#### **Exterior Observations:**

- Exterior guardrail safety barrel not in proper placement (Figure 3)
- Exterior Column SB appears to have cracks in overhead canopy. (Figure 4)
- Exterior outlet encased in concrete crumbling around outlet. (Figure 5)
- Toll booths missing exterior light covers. (Figure 6)

#### **Recommendations:**

- Recommend repairs on exterior concrete items showing cracks and damage. Recommend repairs on guardrail safety attenuator.
- Recommend test and balance of HVAC system in IT room.



**2 - John Young Parkway Ramp – Off Observations:**

The above referenced on ramp is in good condition overall. Maintenance is recommended on some exterior pedestal wiring and exterior exposed conduit.  
(Figure 7, Figure 8, Figure 9 & Figure 10)

**3 - John Young Parkway Ramp – On Observations:**

The above referenced on ramp is in good condition overall. Some maintenance is recommended to ensure exterior ground covers are securely fastened. Maintenance is also recommended on some ceiling tiles and potential leaks need to be addressed.  
(Figure 11, Figure 12, Figure 13 & Figure 14)

**4 – US 441 Ramp – Off Observations:**

The above referenced on ramp is in good condition overall. Some maintenance is recommended to fasten electrical box and unistrut on the camera equipment securely to the building.  
(Figure 15 & Figure 16)

**5 – US 441 Ramp – On Observations:**

The above referenced on ramp is in good condition overall. Maintenance is recommended on some ceiling tiles and potential leaks need to be addressed. Recommend installation of bollards to protect generator.  
(Figure 17, Figure 18, Figure 19 & Figure 20)

**6 – Landstar Ramp – Off Observations:**

The above referenced on ramp is in good condition overall. Some maintenance of isolated wiring and uneven pavement is recommended.  
(Figure 21, Figure 22, Figure 23 & Figure 24)

**7 – Landstar Ramp – On Observations:**

The above referenced on ramp is in good condition overall. Some maintenance on the exterior pedestal, ceiling tiles and potential leaks is recommended.  
(Figure 25, Figure 26 & Figure 27)

All notes and items in this report are a record of observations provided by the site visit. Please find attached to this report, photos and items observed.

Prepared By: Julie A Andrews



Figure 1 John Young Pkwy Mainline IT Room



Figure 2 John Young Pkwy Mainline Tunnel Conduit



Figure 3 John Young Pkwy Mainline Exterior



Figure 4 John Young Pkwy Mainline Exterior



Figure 5 John Young Pkwy Mainline Exterior Outlet



Figure 6 John Young Pkwy Mainline Exterior Booths





Figure 7 John Young Parkway Ramp Off



Figure 8 John Young Parkway Ramp Off



Figure 9 John Young Parkway Ramp Off



Figure 10 John Young Parkway Ramp Off



Figure 11 John Young Parkway Ramp On



Figure 12 John Young Parkway Ramp On





Figure 13 John Young Parkway Ramp On



Figure 14 John Young Parkway Ramp On



Figure 15 US-441 Ramp Off



Figure 16 US-441 Ramp Off



Figure 17 US-441 Ramp On



Figure 18 US-441 Ramp On





Figure 19 US-441 Ramp On



Figure 20 US-441 Ramp On



Figure 21 Landstar Ramp On

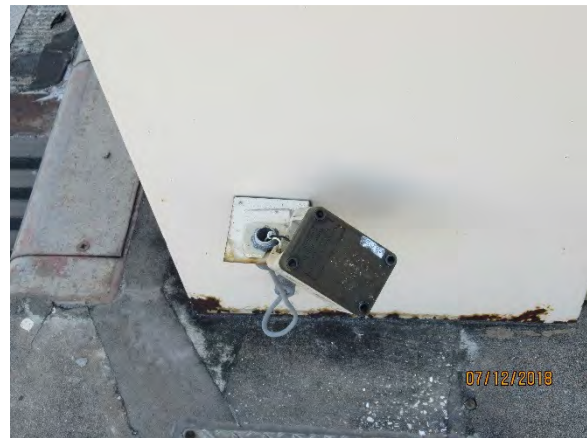


Figure 22 Landstar Ramp On



Figure 23 Landstar Ramp On



Figure 24 Landstar Ramp On



*Figure 25 Landstar Ramp Off*



*Figure 26 Landstar Ramp Off*



*Figure 27 Landstar Ramp Off*





## 2018 Annual Facilities Inspection Report



### Boggy Creek Main Plaza

July 26, 2018

SUBMITTED BY:

**Dewberry Engineers Inc.**

800 N. Magnolia Avenue, Suite 1000  
Orlando, Florida 32803  
407.843.5120

SUBMITTED TO:

**Central Florida Expressway Authority**

Corporate Office  
4974 ORL Tower Road  
Orlando, Florida 32807  
407.690.5000

**Attn: Robert Glasemann  
Facilities Supervisor  
Central Florida Expressway Authority  
4974 ORL Tower Road  
Orlando, FL 32807**

Inspection Date: 07/26/18  
Arrived: 7:30 AM

Dewberry Engineers Inc. (Dewberry) as part of the Annual System Inspection performed the annual building inspection of the Boggy Creek Mainline Plaza and associated ramps.

## **8 – Boggy Creek Main Plaza**

### **Summary of Noted Observations:**

The above referenced site is in good condition overall with no identified deficiencies. Maintenance is recommended on some isolated items as referenced below. Please refer to inspection photos for visual observations. The following items were observed:

#### **Interior Observations:**

- Computer Room wiring under equipment rack. (Figure 1)
- Potential leak from HVAC equipment (Figure 2)
- Ceiling tiles showing potential roof leaks (Figure 3 & Figure 4)

#### **Tunnel Observations:**

- Mainline tunnel seams are showing deterioration.  
(Figure 5 Figure 6, Figure 7 & Figure 8)

#### **Exterior Observations:**

- Exposed rebar in exterior landscaping area. (Figure 9 & Figure 10)
- Building façade showing separation from building. (Figure 11 & Figure 12)

#### **Recommendations:**

- Recommend maintenance on building façade to address separation from building.
- Recommend testing on HVAC to ensure unit is working properly.
- Recommend examination of roof to locate any potential leaks.
- Recommend general maintenance in tunnel to repair noted deficiencies.

**9 – Boggy Creek Ramp – Off Observations:**

The above referenced on ramp is in good condition overall. Maintenance to ensure exterior equipment covers are secured properly is recommended. Maintenance is recommended on some ceiling tiles and potential leaks need to be addressed.  
(Figure 13, Figure 14, Figure 15 & Figure 16)

**10 – Boggy Creek Ramp – On Observations:**

The above referenced on ramp is in good condition overall. Some maintenance on the exterior pedestal, ceiling tiles and potential leaks is recommended. Recommend installation of bollards to protect generator location. (Figure 17, Figure 18, Figure 19 & Figure 20)

**11 – South Access Ramp – On Observations:**

The above referenced on ramp is in good condition overall. Maintenance of the potential trip hazard is recommended. (Figure 21 & Figure 22)

**12 – Lake Nona Ramp – Off Observations:**

The above referenced on ramp is in good condition overall. Maintenance on the overhead sign is recommended. (Figure 23 & Figure 24)

**13 – Lake Nona Ramp – On Observations:**

The above referenced on ramp is in good condition overall. Some maintenance of isolated wiring on the outlet missing power is recommended. (Figure 25 & Figure 26)

**14 – Narcoossee Ramp – Off Observations:**

The above referenced on ramp is in good condition overall. Minor maintenance to attach the loose pedestal cover and replace the vent cover is recommended.  
(Figure 27, Figure 28, Figure 29 & Figure 30)

**15 – Narcoossee Ramp – On Observations:**

The above referenced on ramp is in good condition overall. Some maintenance of the installation of the wiring across ceiling tiles to the equipment rack, ceiling tiles and potential leaks is recommended. (Figure 31, Figure 32, Figure 33 & Figure 34)

**16 – Moss Park Ramp – Off Observations:**

The above referenced on ramp is in good condition overall. Some maintenance to patch the tears in the ductwork, ceiling tiles and potential leaks need to be addressed. Recommend installation of bollards to protect generator location. (Figure 35, Figure 36, Figure 37 & Figure 38)

**17 – Moss Park Ramp – On Observations:**

The above referenced on ramp is in good condition overall. Some maintenance of isolated wiring on the outlet missing power, ceiling tiles and potential leaks is recommended. Recommend installation of bollards to protect generator location is recommended.  
(Figure 39, Figure 40, Figure 41 & Figure 42)

**18 – Innovation Way Ramp – Off Observations:**

The above referenced on ramp is in good condition overall. Some Maintenance to check the generator set is recommended. (Figure 43 & Figure 44)

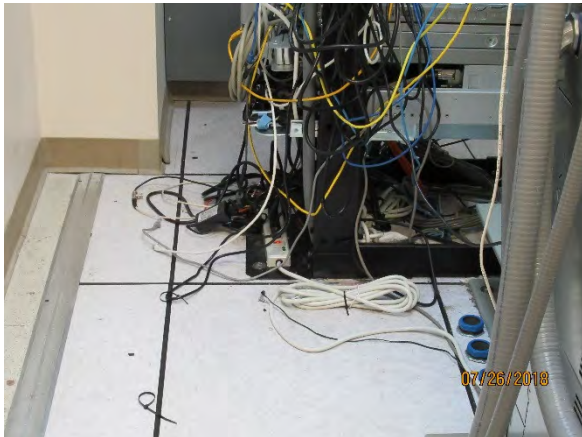
**19 – Innovation Way Ramp – On Observations:**

The above referenced on ramp is in good condition overall. Some maintenance to test and balance the HVAC system is recommended.  
(Figure 45, Figure 46, Figure 47 & Figure 48)



All notes and items in this report are a record of observations provided by the site visit. Please find attached to this report, photos and items observed.

Prepared By: Julie A Andrews



*Figure 1 Boggy Creek Mainline*



*Figure 2 Boggy Creek Mainline*



*Figure 3 Boggy Creek Mainline*



*Figure 4 Boggy Creek Mainline*



*Figure 5 Boggy Creek Mainline Tunnel*



*Figure 6 Boggy Creek Mainline Tunnel*





*Figure 7 Boggy Creek Mainline Tunnel*



*Figure 8 Boggy Creek Mainline Tunnel*



*Figure 9 Boggy Creek Mainline Exterior Exposed Rebar*



*Figure 10 Boggy Creek Mainline Exterior Exposed Rebar*



*Figure 11 Boggy Creek Mainline Exterior*



*Figure 12 Boggy Creek Mainline Exterior*





Figure 13 Boggy Creek Ramp Off



Figure 14 Boggy Creek Ramp Off



Figure 15 Boggy Creek Ramp Off



Figure 16 Boggy Creek Ramp Off



Figure 17 Boggy Creek Ramp On



Figure 18 Boggy Creek Ramp On





Figure 19 Boggy Creek Ramp On



Figure 20 Boggy Creek Ramp On



Figure 21 South Access Ramp Off (Boggy New)



Figure 22 South Access Ramp Off (Boggy New)

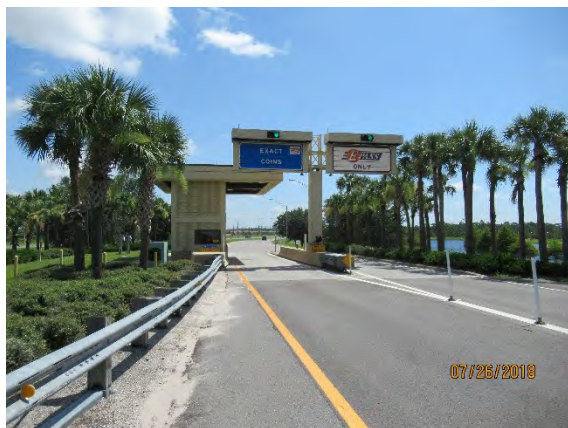


Figure 23 Lake Nona Ramp On



Figure 24 Lake Nona Ramp On





Figure 25 Lake Nona Ramp Off



Figure 26 Lake Nona Ramp Off



Figure 27 Narcoossee Ramp Off



Figure 38 Narcoossee Ramp Off



Figure 29 Narcoossee Ramp Off



Figure 30 Narcoossee Ramp Off





Figure 31 Narcoossee Ramp On



Figure 32 Narcoossee Ramp On



Figure 33 Narcoossee Ramp On



Figure 34 Narcoossee Ramp On



Figure 35 Moss Park Ramp Off



Figure 36 Moss Park Ramp Off





Figure 37 Moss Park Ramp Off



Figure 38 Moss Park Ramp Off



Figure 39 Moss Park Ramp On



Figure 40 Moss Park Ramp On



Figure 41 Moss Park Ramp On



Figure 42 Moss Park Ramp On





Figure 43 Innovation Ramp Off



Figure 44 Innovation Ramp Off



Figure 45 Innovation Way Ramp On



Figure 46 Innovation Way Ramp On



Figure 47 Innovation Way Ramp On



Figure 48 Innovation Way Ramp On



## 2018 Annual Facilities Inspection Report



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### Curry Ford Main Plaza

August 2, 2018

SUBMITTED BY:

**Dewberry Engineers Inc.**

800 N. Magnolia Avenue, Suite 1000  
Orlando, Florida 32803  
407.843.5120

SUBMITTED TO:

**Central Florida Expressway Authority**

Corporate Office  
4974 ORL Tower Road  
Orlando, Florida 32807  
407.690.5000

**Attn: Robert Glasemann  
Facilities Supervisor  
Central Florida Expressway Authority  
4974 ORL Tower Road  
Orlando, FL 32807**

Inspection Date: 09/02/18  
Arrived: 7:30 AM

Dewberry Engineers Inc. (Dewberry) was called to perform the annual building inspection of the Curry Ford Mainline Plaza and associated ramps.

## **22 – Curry Ford Main Plaza**

### **Summary of Noted Observations:**

The above referenced site is in good condition overall with no identified deficiencies. Maintenance is recommended on some isolated items as referenced below. Please refer to inspection photos for visual observations. The following items were observed:

#### **Interior Observations:**

- Computer room wiring under equipment rack. (Figure 1)
- Main plaza stairwell tunnel duct detector wiring fastened to duct ceiling thread. (Figure 2)

#### **Tunnel Observations:**

- Main Plaza tunnel ceiling seam/wood separation from ceiling. (Figure 3)
- Main Plaza tunnel has significant water leaking throughout the tunnel. (Figure 4 & Figure 5)

#### **Exterior Observations:**

- Building façade is showing separation from building. (Figure 7 & Figure 8)

#### **Recommendations:**

- Recommend maintenance on building façade to address separation from building
- Recommend general maintenance in tunnel to repair noted deficiencies.
- Recommend HVAC contractor to ensure all vents are clear and have vent covers.
- Recommend electrical contractor check the wiring issues and repair as required.



**20 – Lee Vista Ramp – On Observations:**

The above referenced on ramp is in good condition overall. Replacement of missing vents in duct work and maintenance on building façade showing separation from building is recommended. (Figure 9 & Figure 10)

**21 – Lee Vista Ramp – Off Observations:**

The above referenced on ramp is in good condition overall. Replacement of missing generator cover and maintenance on building façade showing separation from building is recommended. (Figure 11 & Figure 12)

**23 – Curry Ford Ramp – Off Observations:**

The above referenced on ramp is in good condition overall. Maintenance on UPS is recommended. (Figure 13 & Figure 14)

**24 – Curry Ford Ramp – On Observations:**

The above referenced on ramp is in good condition overall. Maintenance to repair counting machine separation from wall is recommended. (Figure 15, Figure 16, Figure 17 & Figure 18)

All notes and items in this report are a record of observations provided by the site visit. Please find attached to this report, photos and items observed.

Prepared By: Julie A Andrews



Figure 1 Curry Ford Mainline IT Room



Figure 2 Curry Ford Mainline Stairwell



Figure 3 Curry Ford Mainline Tunnel



Figure 4 Curry Ford Mainline Tunnel



Figure 5 Curry Ford Mainline Tunnel



Figure 6 Curry Ford Mainline Toll Booth





Figure 7 Lee Vista Ramp On



Figure 8 Lee Vista Ramp On



Figure 9 Lee Vista Ramp On



Figure 10 Lee Vista Ramp On

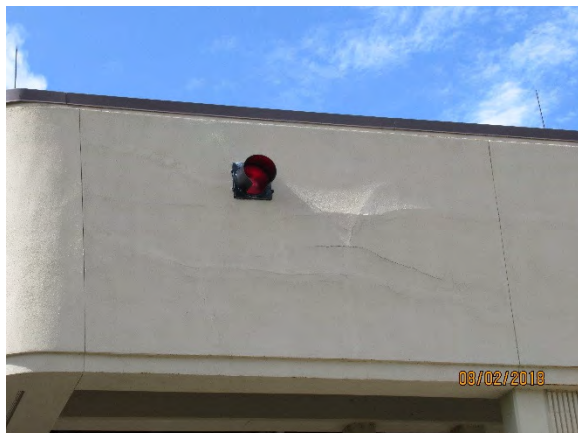


Figure 11 Lee Vista Ramp Off



Figure 12 Lee Vista Ramp Off





Figure 13 Curry Ford Ramp Off



Figure 14 Curry Ford Ramp Off



Figure 15 Curry Ford Ramp On



Figure 16 Curry Ford Ramp On



Figure 17 Curry Ford Ramp On



Figure 18 Curry Ford Ramp On

## 2018 Annual Facilities Inspection Report



### University Main Plaza

August 9, 2018

SUBMITTED BY:

**Dewberry Engineers Inc.**  
800 N. Magnolia Avenue, Suite 1000  
Orlando, Florida 32803  
407.843.5120

SUBMITTED TO:

**Central Florida Expressway Authority**  
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4974 ORL Tower Road  
Orlando, Florida 32807  
407.690.5000

**Attn: Robert Glasemann  
Facilities Supervisor  
Central Florida Expressway Authority  
4974 ORL Tower Road  
Orlando, FL 32807**

Inspection Date: 08/09/18  
Arrived: 7:30 AM

Dewberry Engineers Inc. (Dewberry) as part of the Annual System Inspection performed the annual building inspection of the University Creek Mainline Plaza and associated ramps.

## **27 – University Main Plaza**

### **Summary of Noted Observations:**

The above referenced site is in good condition overall with no identified deficiencies. Maintenance is recommended on some isolated items as referenced below. Please refer to inspection photos for visual observations. The following items were observed:

#### **Interior Observations:**

- Ceiling vent missing cover in computer room. (Figure 1)
- Manager office outlet and wiring separated from wall. (Figure 2)

#### **Tunnel Observations:**

- Mainline tunnel seams are showing deterioration. (Figure 3)
- Mainline tunnel ductwork showing separation (Figure 4)

#### **Exterior Observations:**

- Building façade showing cracks and minor separation from building. (Figure 5 & Figure 6)

#### **Recommendations:**

- Recommend maintenance on building façade to address separation from building.
- Recommend examination of roof to locate any potential leaks.
- Recommend general maintenance in tunnel to repair noted deficiencies.



**25 – Colonial Ramp – On Observations:**

The above referenced on ramp is in good condition overall. Pest control is recommended to address the wasp nests on building façade. Maintenance to repair door latch and attach Trans Core enclosure to wall is also recommended. (Figure 7, Figure 8, Figure 9 & Figure 10)

**26 – Colonial Ramp – Off Observations:**

The above referenced on ramp is in good condition overall. Some maintenance on damaged guard rail and a permanent solution to the E-Pass lane is recommended.  
(Figure 11, Figure 12, Figure 13 & Figure 14)

**28 – University Ramp – On Observations:**

The above referenced on ramp is in good condition overall. Some maintenance to install missing ceiling tiles, repair leaks in ceiling and remove parking stop from landscape is recommended.  
(Figure 15, Figure 16, Figure 17 & Figure 18)

**29 – University Ramp – Off Observations:**

The above referenced on ramp is in good condition overall. Some maintenance to install missing exterior equipment covers and UPS is recommended. (Figure 9 & Figure 10)

All notes and items in this report are a record of observations provided by the site visit. Please find attached to this report, photos and items observed.

Prepared By: Julie A Andrews



Figure 1 University Mainline IT Room



Figure 2 University Mainline Manager's Office



Figure 3 University Mainline Tunnel Seam



Figure 4 University Mainline Tunnel Ductwork



Figure 5 University Mainline Exterior

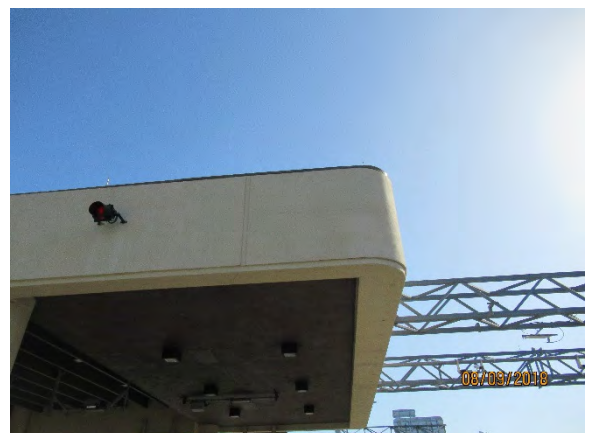


Figure 6 University Mainline Exterior





Figure 7 Colonial Ramp On



Figure 8 Colonial Ramp On



Figure 9 Colonial Ramp On

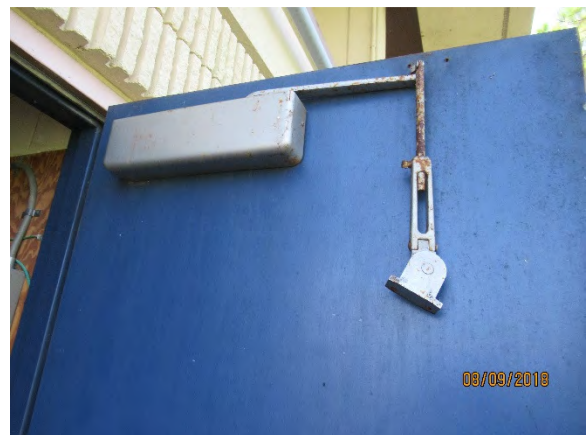


Figure 10 Colonial Ramp On



Figure 11 Colonial Ramp Off



Figure 12 Colonial Ramp Off





Figure 13 Colonial Ramp Off



Figure 14 Colonial Ramp Off



Figure 15 University Ramp Off



Figure 16 University Ramp Off



Figure 17 University Ramp Off



Figure 18 University Ramp Off





Figure 19 University Ramp On



Figure 20 University Ramp On



Figure 21 University Ramp On



Figure 22 University Ramp On



## 2018 Annual Facilities Inspection Report



### Independence Main Plaza

September 20, 2018

SUBMITTED BY:

**Dewberry Engineers Inc.**

800 N. Magnolia Avenue, Suite 1000  
Orlando, Florida 32803  
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SUBMITTED TO:

**Central Florida Expressway Authority**

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**Attn: Robert Glasemann  
Facilities Supervisor  
Central Florida Expressway Authority  
4974 ORL Tower Road  
Orlando, FL 32807**

Inspection Date: 09/20/18  
Arrived: 7:30 AM

Dewberry Engineers Inc. (Dewberry) as part of the Annual System Inspection performed the annual building inspection of the Independence Mainline Plaza and associated ramps.

## **76 – Independence Main Plaza**

### **Summary of Noted Observations:**

The above referenced site is in good condition overall with no identified deficiencies. Maintenance is recommended on some isolated items as referenced below. Please refer to inspection photos for visual observations. The following items were observed:

#### **Interior Observations:**

- Mainline computer room has isolated wiring hanging across the room. (Figure 1 & Figure 2)
- Mainline computer room is missing vent cover. (Figure 3)
- Mainline ductwork has tear and potential leak. (Figure 4)

#### **Overpass Observations:**

- Overpass area is in need of pressure washing. (Figure 5 & Figure 6)

#### **Exterior Observations:**

- Building façade showing cracks and minor separation from building. (Figure 8)

#### **Recommendations:**

- Recommend pressure wash overpass area.
- Recommend maintenance on building façade to address separation from building.
- Recommend examination of roof to locate any potential leaks.

**72 – Schofield Ramp – On Observations:**

The above referenced on ramp is in good condition overall. Maintenance on ceiling tiles to locate potential is recommended. (Figure 11 & Figure 12)

**73 – Schofield Ramp – Off Observations:**

The above referenced on ramp is in good condition overall. Maintenance to repair tears in the duct work and landscaping to remove vines from rack is recommended.  
(Figure 13, Figure 14, Figure 15 & Figure 16)

**74 – New Independence Ramp – On Observations:**

The above referenced on ramp is in good condition overall. Maintenance to address the peeling paint on the signage post is recommended. (Figure 17 & Figure 18)

**75 – New Independence Ramp – Off Observations:**

The above referenced on ramp is in good condition overall. Maintenance to address the peeling paint on the signage post is recommended. (Figure 19, Figure 20, Figure 21 & Figure 22)

**77 – CR-535 Ramp – Off Observations:**

The above referenced on ramp is in good condition overall. Maintenance to repair building façade showing separation from building and pedestal wiring is recommended.  
(Figure 23, Figure 24, Figure 25 & Figure 26)

**78 – CR-535 Ramp – On Observations:**

The above referenced on ramp is in good condition overall. (Figure 27 & Figure 28)

All notes and items in this report are a record of observations provided by the site visit. Please find attached to this report, photos and items observed.

Prepared By: Julie A Andrews



Figure 1 Independence Mainline



Figure 2 Independence Mainline



Figure 3 Independence Mainline



Figure 4 Independence Mainline



Figure 5 Independence Mainline



Figure 6 Independence Mainline





Figure 7 Independence Mainline



Figure 8 Independence Mainline



Figure 9 Independence Mainline



Figure 10 Independence Mainline



Figure 11 Schofield Ramp On



Figure 12 Schofield Ramp On



Figure 13 Schofield Ramp Off



Figure 14 Schofield Ramp Off

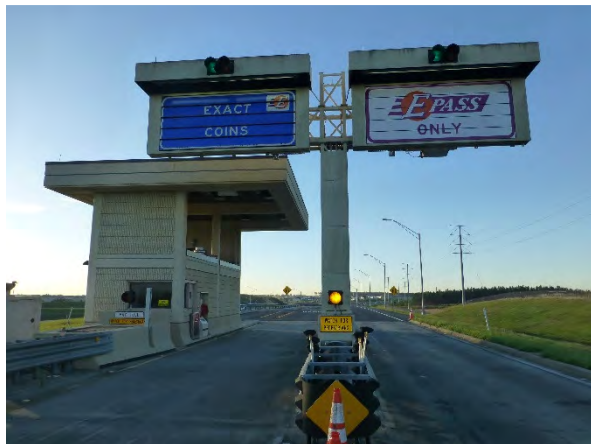


Figure 15 Schofield Ramp On



Figure 16 Schofield Ramp On



Figure 17 New Independence Ramp Off



Figure 18 New Independence Ramp Off





Figure 19 New Independence Ramp On



Figure 20 New Independence Ramp On



Figure 21 New Independence Ramp On



Figure 22 New Independence Ramp On



Figure 23 CR-535 Ramp Off

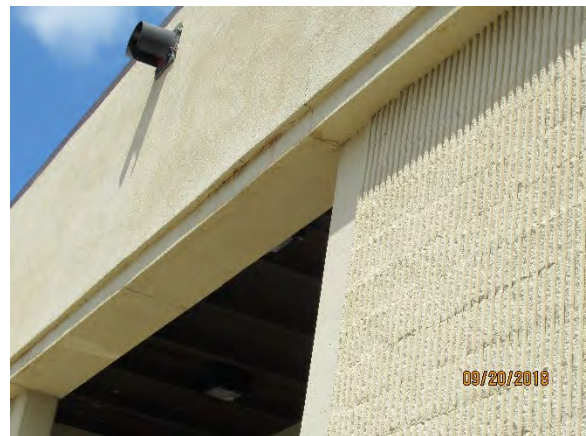


Figure 24 CR-535 Ramp Off





*Figure 25 CR-535 Ramp Off*



*Figure 26 CR-535 Ramp Off*



*Figure 27 CR-535 Ramp On*



*Figure 28 CR-535 Ramp On*



## 2018 Annual Facilities Inspection Report



### Forest Lake Main Plaza

September 27, 2018

SUBMITTED BY:

**Dewberry Engineers Inc.**

800 N. Magnolia Avenue, Suite 1000  
Orlando, Florida 32803  
407.843.5120

SUBMITTED TO:

**Central Florida Expressway Authority**

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Orlando, Florida 32807  
407.690.5000



**Attn: Robert Glasemann  
Facilities Supervisor  
Central Florida Expressway Authority  
4974 ORL Tower Road  
Orlando, FL 32807**

Inspection Date: 09/27/18  
Arrived: 7:00 AM

Dewberry Engineers Inc. (Dewberry) as part of the Annual System Inspection performed the annual building inspection of the Forest Lake Mainline Plaza and associated ramps.

### **83 – Forest Lake Main Plaza**

#### **Summary of Noted Observations:**

The above referenced site is in good condition overall with no identified deficiencies. Maintenance is recommended on some isolated items as referenced below. Please refer to inspection photos for visual observations. The following items were observed:

#### **Interior Observations:**

- Mainline has identified leaks above ceiling tile in computer room, break room and recording room. (Figure 1, Figure 2, Figure 4, Figure 7 & Figure 8)
- Mainline computer room is missing vent cover above equipment racks. (Figure 3)

#### **Overpass Observations:**

- Overpass has some recommended areas of repair in seam as shown in inspection photos. (Figure 5 & Figure 6)

#### **Exterior Observations:**

- Exterior toll booths have some isolated wiring issues identified and missing power outlets. (Figure 9 & Figure 10)
- Building façade showing separation from building. (Figure 11 & Figure 12)

#### **Recommendations:**

- Recommend maintenance on building façade to address separation from building
- Recommend maintenance on overpass to restore and repaint identified area in need of repair.
- Recommend maintenance to determine the cause of the leaks in the ceiling tiles.

**79 – SR-438 Ramp – On Observations:**

The above referenced on ramp is in good condition overall. Maintenance to locate potential ceiling leaks is recommended (Figure 13 & Figure 14)

**80 – SR-438 Ramp – Off Observations:**

The above referenced on ramp is in good condition overall. Some maintenance to test the overpass sign is recommended. (Figure 15 & Figure 16)

**81 – West Road Ramp – On Observations:**

The above referenced on ramp is in good condition overall. (Figure 17 & Figure 18)

**82 – West Road Ramp – Off Observations:**

The above referenced on ramp is in good condition overall. (Figure 19 & Figure 20)

**84 – CR 437 Ramp – On Observations:**

The above referenced on ramp is in good condition overall. Some isolated wiring to outlets without power needs to be addressed. (Figure 21 & Figure 22)

**85 – CR 437 Ramp – Off Observations:**

The above referenced on ramp is in good condition overall. Maintenance to securely attach outdoor equipment cover is recommended. (Figure 23 & Figure 24)

**86 – Ponkan Gantry – Observations:**

The above referenced on ramp is in good condition overall. (Figure 25 & Figure 26)

**87 – Coronado Gantry – Observations:**

The above referenced on ramp is in good condition overall. (Figure 27 & Figure 28)

**88 – Mt. Plymouth Gantry – Observations:**

The above referenced on ramp is in good condition overall. (Figure 29 & Figure 30)

All notes and items in this report are a record of observations provided by the site visit. Please find attached to this report, photos and items observed.

Prepared By: Julie A Andrews





Figure 1 Forest Lake Mainline IT Room



Figure 2 Forest Lake Mainline IT Room



Figure 3 Forest Lake Mainline IT Room



Figure 4 Forest Lake Mainline Break Room



Figure 5 Forest Lake Mainline



Figure 6 Forest Lake Mainline



Figure 7 Forest Lake Mainline (Opposite)



Figure 8 Forest Lake Mainline (Opposite)

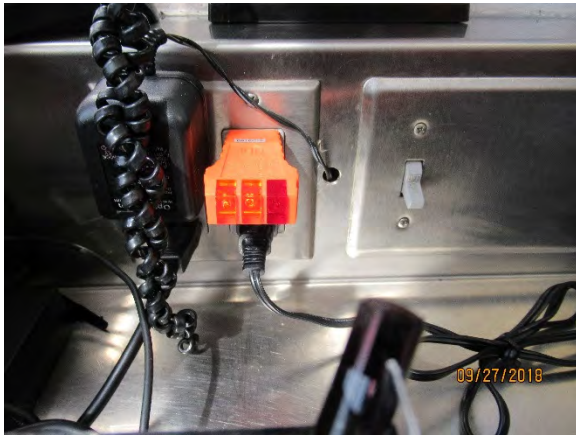


Figure 9 Forest Lake Mainline



Figure 10 Forest Lake Mainline



Figure 11 Forest Lake Mainline



Figure 12 Forest Lake Mainline





Figure 13 SR-438 Ramp On



Figure 14 SR-438 Ramp On



Figure 15 SR-438 Ramp Off



Figure 16 SR-438 Ramp Off



Figure 17 West Road Ramp On



Figure 18 West Road Ramp On





Figure 19 West Road Ramp Off



Figure 20 West Road Ramp Off



Figure 21 CR-437A Ramp Off



Figure 22 CR-437A Ramp Off



Figure 23 CR-437A Ramp On



Figure 24 CR-437A Ramp On





Figure 25 Ponkan Gantry



Figure 26 Ponkan Gantry



Figure 27 Coronado Gantry



Figure 28 Coronado Gantry



Figure 29 Mt. Plymouth Gantry



Figure 30 Mt. Plymouth Gantry



## 2018 Annual Facilities Inspection Report



### Beachline Main Plaza

August 30, 2018

SUBMITTED BY:

**Dewberry Engineers Inc.**

800 N. Magnolia Avenue, Suite 1000  
Orlando, Florida 32803  
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SUBMITTED TO:

**Central Florida Expressway Authority**

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4974 ORL Tower Road  
Orlando, Florida 32807  
407.690.5000

**Attn: Robert Glasemann  
Facilities Supervisor  
Central Florida Expressway Authority  
4974 ORL Tower Road  
Orlando, FL 32807**

Inspection Date: 08/30/18  
Arrived: 7:30 AM

Dewberry Engineers Inc. (Dewberry) as part of the Annual System Inspection performed the annual building inspection of the Beachline Young Parkway Mainline Plaza and associated ramps.

**35 – Beachline Main Plaza**

**Summary of Noted Observations:**

The above referenced site is in good condition overall with no identified deficiencies. Maintenance is recommended on some isolated items as referenced below. Please refer to inspection photos for visual observations. The following items were observed:

**Interior Observations:**

- Mainline computer room has isolated wiring bundles with multiple power strips on floor below equipment racks. (Figure 1, Figure 2 & Figure 3)
- Mainline outlet in counting room is hanging from wall. (Figure 4)

**Tunnel Observations:**

- The above referenced tunnel is in good condition overall with no identified deficiencies.

**Exterior Observations:**

- Exterior building is in good condition overall.
- Portable generator located behind mainline building. (Figure 5 & Figure 6)

**Recommendations:**

- Recommend maintenance on of isolated wiring for computer room.
- Recommend electrical contractor determine computer room wiring can be addressed.
- Recommend determining solution to alleviate need for portable generator.



**30 – McCoy Ramp – Off Observations:**

The above referenced off ramp is in good condition overall. (Figure 9 & Figure 10)

**31 – Jetport Ramp – On Observations:**

The above referenced on ramp is in good condition overall. (Figure 11 & Figure 12)

**32 – Conway Ramp – Off Observations:**

The above referenced off ramp is in good condition overall. Maintenance to ensure underground propane tank cover is securely fastened. (Figure 13 & Figure 14)

**33 – Tradeport Ramp – On Observations:**

The above referenced on ramp is in good condition overall. Maintenance is recommended on ceiling tiles and potential leaks need to be addressed. (Figure 15 & Figure 16)

**36 – ICP Ramp – Off Observations:**

The above referenced on ramp is in good condition overall. (Figure 17 & Figure 18)

**37 – ICP Ramp – Off Observations:**

The above referenced on ramp is in good condition overall. (Figure 19 & Figure 20)

All notes and items in this report are a record of observations provided by the site visit. Please find attached to this report, photos and items observed.

Prepared By: Julie A Andrews



Figure 1 Beachline Mainline



Figure 2 Beachline Mainline



Figure 3 Beachline Mainline



Figure 4 Beachline Mainline



Figure 5 Beachline Mainline



Figure 6 Beachline Mainline





Figure 7 Beachline Mainline



Figure 8 Beachline Mainline



Figure 9 McCoy Road Ramp Off



Figure 10 McCoy Ramp Off



Figure 11 Jetport Ramp On



Figure 12 Jetport Ramp On





*Figure 13 Conway Ramp On*



*Figure 14 Conway Ramp On*



*Figure 15 Tradeport Ramp On*



*Figure 16 Tradeport Ramp On*



*Figure 17 ICP Ramp Off*



*Figure 18 ICP Ramp Off*





*Figure 19 ICP Ramp On*



*Figure 20 ICP Ramp On*

## 2018 Annual Facilities Inspection Report



### Dallas Main Plaza

August 16, 2018

SUBMITTED BY:

**Dewberry Engineers Inc.**

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SUBMITTED TO:

**Central Florida Expressway Authority**

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Orlando, Florida 32807  
407.690.5000



**Attn: Robert Glasemann  
Facilities Supervisor  
Central Florida Expressway Authority  
4974 ORL Tower Road  
Orlando, FL 32807**

Inspection Date: 08/16/18  
Arrived: 7:00 AM

Dewberry Engineers Inc. (Dewberry) as part of the Annual System Inspection performed the annual building inspection of the Dallas Mainline Plaza and associated ramps.

### **38 – Dallas Main Plaza**

#### **Summary of Noted Observations:**

The above referenced site is in good condition overall with no identified deficiencies. Maintenance is recommended on some isolated items as referenced below. Please refer to inspection photos for visual observations. The following items were observed:

#### **Interior Observations:**

- Mainline conference room ceiling tile has potential leak. (Figure 1)
- Mainline computer room has isolated wiring bundles with multiple power strips on floor below equipment racks. (Figure 2)
- Stairwell hardware is showing separation from interior wall. (Figure 5 & Figure 6)

#### **Overpass Observations:**

- Mainline overpass is in need of pressure wash. (Figure 3 & Figure 4)

#### **Exterior Observations:**

- Exterior building has an abundance of wasp nests on the building and inside electrical room. (Figure 7 & Figure 8)

#### **Recommendations:**

- Recommend pest control for this location.
- Recommend pressure wash for the building exterior and overpass.
- Recommend maintenance to locate/address leaks in ceiling tile.

**39 – Dallas Ramp – On Observations:**

The above referenced on ramp is in good condition overall. Action ice machine using ramp power source. (Figure 11 & Figure 12)

**40 – Dallas Ramp – Off Observations:**

The above referenced on ramp is in good condition overall. (Figure 9 & Figure 10)

All notes and items in this report are a record of observations provided by the site visit. Please find attached to this report, photos and items observed.

Prepared By: Julie A Andrews





Figure 1 Dallas Mainline Conference Room



Figure 2 Dallas Mainline Overpass



Figure 3 Dallas Mainline Overpass



Figure 4 Dallas Mainline



Figure 5 Dallas Mainline



Figure 6 Dallas Mainline





Figure 7 Dallas Mainline



Figure 8 Dallas Mainline



Figure 9 Dallas Ramp On



Figure 10 Dallas Ramp Off



Figure 11 Dallas Ramp Off



Figure 12 Dallas Ramp Off



*Figure 13 Dallas Ramp On*





## 2018 Annual Facilities Inspection Report



### Goldenrod Main Plaza (Non-System Facility)

August 2, 2018

SUBMITTED BY:

**Dewberry Engineers Inc.**

800 N. Magnolia Avenue, Suite 1000  
Orlando, Florida 32803  
407.843.5120

SUBMITTED TO:

**Central Florida Expressway Authority**

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4974 ORL Tower Road  
Orlando, Florida 32807  
407.690.5000

**Attn: Robert Glasemann  
Facilities Supervisor  
Central Florida Expressway Authority  
4974 ORL Tower Road  
Orlando, FL 32807**

Inspection Date: 08/02/18  
Arrived: 10:00 AM

Dewberry Engineers Inc. (Dewberry) as part of the Annual System Inspection performed the annual building inspection of the Goldenrod Mainline Plaza.

### **34 – Goldenrod Main Plaza**

#### **Summary of Noted Observations:**

The above referenced site is in good condition overall with no identified deficiencies. Maintenance is recommended on some isolated items as referenced below. Please refer to inspection photos for visual observations. The following items were observed:

#### **Interior Observations:**

- Mainline computer room has electrical outlet missing power and electrical box missing cover.  
(Figure 2 & Figure 3)

#### **Recommendations:**

- Recommend electrical contractor provide general repair and replace missing outlet cover.

All notes and items in this report are a record of observations provided by the site visit. Please find attached to this report, photos and items observed.

Prepared By: Julie A Andrews



*Figure 1 Goldenrod Main Plaza Computer Room*



*Figure 2 Goldenrod Main Plaza*



*Figure 3 Goldenrod Main Plaza*



